

Rezoning Transportation Analysis

Petition Number: 2025-100

General Location Identifier: 14922399

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Reviewer:

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Revision Log:

Date	Description
9-24-25	First Review (NK)
10-23-25	Second Review (NK)

General Review Information

The petition is located adjacent to Marsh Road, a City-maintained major collector, north of Selwyn Farms Lane, a City-maintained local street. The petition is located in a south wedge inside of Route 4, within Park Woodlawn Area Plan.

Active Projects Near the Site:

- N/A

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located adjacent to Marsh Road, a City-maintained major collector, north of Selwyn Farms Lane, a City-maintained local street. A Traffic Impact Study (TIS) is not required for the site due to the development generating site trips under ordinance thresholds and not meeting additional TIS triggers. Site conditional note revisions are needed to fix a minor error. Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation			Source
			Daily Trips	AM Peak Hour Trips	PM Peak Hour Trips	
Existing Use	Daycare	9,153 SF	436	101	102	<i>Tax Record</i>
Entitlement with Current Zoning	Daycare (R-12 MF, 3.16 acres)	9,153 SF	436	101	102	<i>General Guidance from Planning & RZ# 1999-077</i>
Proposed Zoning	Multifamily Attached (N2-A, 3.16 acres)	38 DUs	173	14	15	Site Plan: 08-13-25
Proposed Zoning	Multifamily Attached (N2-A, 3.16 acres)	37 DUs	168	14	14	Site Plan: 10-13-25

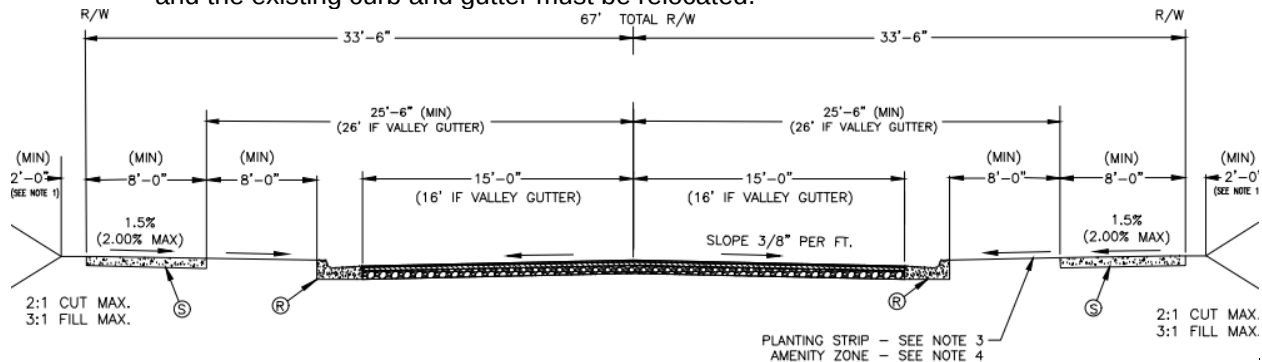
Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

Outstanding Issues

~~Strikethrough = Resolved~~

1. ~~Curbline:~~

- a. ~~Marsh Road: The location of the future back of curb and gutter is 17.5' from road centerline, and the existing curb and gutter must be relocated.~~



U-07B Local Collector Street Typical Section (8' Sidewalk)

Label and dimension the curb and gutter from the centerline for each road on the site plan.

2. ~~Comprehensive Transportation Review (CTR):~~

~~A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating site trips under ordinance thresholds found in the Charlotte Streets manual Table 3.1 and/or not triggering any other City TIS requirements.~~

3. ~~Revise site plan and conditional note(s) to commit to dedicate 33.5' right-of-way from the road centerline. The site plan shall label and dimension the right-of-way from the road centerline.~~
4. ~~Add a conditional note specifying "A Right of Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick~~

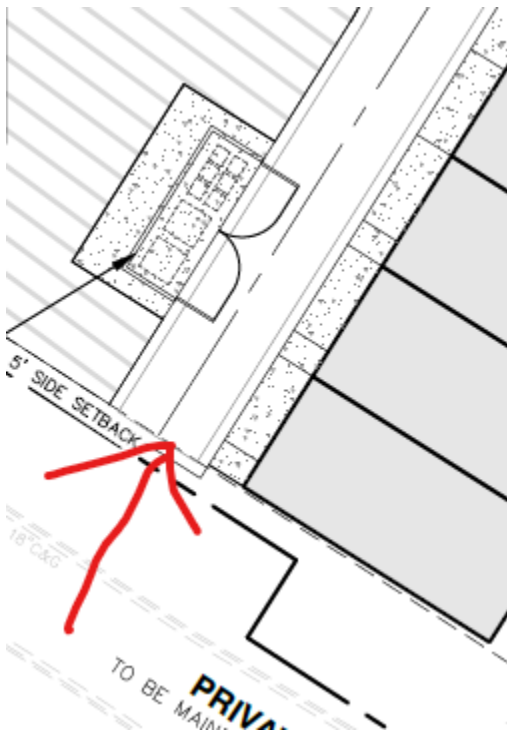
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pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements."

5. ~~Revise site plan and conditional note(s) to commit to installing an 8' sidewalk and 8' planting strip along Marsh Road. Callout infrastructure on the site plan and dimension from roadway centerline. Add a conditional note committing to this improvement.~~
6. ~~Revise site plan to label internal streets as public, private, or private alley. Additional comments may be generated depending on classification of internal street network.~~
7. ~~Revise site plan and conditional note(s) to commit to verifying that there is adequate space between median island and security gate, so no maneuvering is done within the RAW for vehicle turnarounds.~~
8. ~~Revise site plan and conditional note(s) to commit to ensuring that a minimum of 40' is provided between any card reader and the back of sidewalk.~~
9. **Comment Rescinded 10.22.25:** ~~Revise site plan to provide a residential hammerhead at street dead ends. Example indicated below.~~



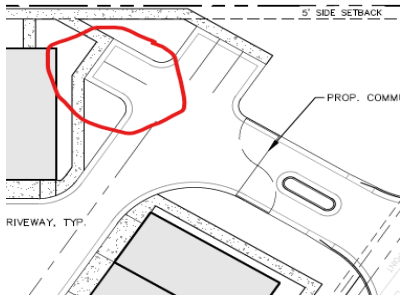
Petitioner specified that the streets identified were private and not network required.

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10. ~~Revise site plan to ensure that vehicles parking in circled area can reverse and turnaround before the proposed community gate.~~



11. ~~Add a conditional note specifying "the Petitioner shall dedicate and convey in fee simple all rights of way to the City before the site's first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible."~~

12. ~~Add a conditional note specifying "All transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued."~~

13. **New Comment 10.22.25:** Clarify highlighted part shown in note below.

If a security gate is provided, adequate space shall allow for maneuvering outside the right-of-way for vehicle turnarounds and a minimum 40' shall be provided between any card reader and the back of sidewalk.

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte Unified Development Ordinance.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and to comply with City of Charlotte Unified Development Ordinance regulations.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>