

Rezoning Transportation Analysis

Petition Number: 2025-091

General Location Identifier: 14110107, 141101013

From: Jake Carpenter, PE

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980-221-5675

Reviewer:

Chris Manno

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980-408-0281

Revision Log:

Date	Description
08-28-25	First Review (CM)
11-20-25	Second Review (CM)

General Review Information

The petition is adjacent to Steele Creek Road, a State-maintained major arterial, north of Shopton Road, a State-maintained, minor arterial. The petition is located in an industrial activity center outside of Route 4, within the I-485 Interchange Analysis.

Active Projects Near the Site:

- TIP - Steele Creek Road (NC 160)
 - Project Limits: I-485 to Western Pkwy.
 - TIP Number: Not applicable
 - Advanced Project Description: Widen from 4 lanes to 6 lanes, with median and multi-use path
 - ROW Year: Undetermined
 - Construction Year: Undetermined

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is adjacent to Steele Creek Road, a State-maintained major arterial, north of Shopton Road, a State-maintained, minor arterial. A Traffic Impact Study (TIS) is not required for the site due to the development generating site trips under ordinance thresholds and not meeting additional TIS triggers. Site plan and/or conditional note revisions are needed to revise ROW along Steele Creek Road to minimum of back of sidewalk, provide minimum 100ft stem into site off Steele Creek Road, add 10x10 sight triangles at all driveways, adjust on-street parking location from driveways and intersections, show 2ft ROW or SUE behind all public sidewalks, revise driveway along Private Alley #1 to avoid sight triangle, align Red Spring Drive to match existing centerline, revise site plan to shift buildings and seating areas out of ROW/SUE, in accordance with the UDO. Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation			Source
			Daily Trips	AM Peak Hour Trips	PM Peak Hour Trips	
Existing Use	Vacant	-	-	-	-	<i>Tax Record</i>
Entitlement with Current Zoning	Hotel (O-2, 6.97 acres)	91 Rooms	563	38	39	<i>General Guidance from Planning & RZ# 2011-056</i>
Proposed Zoning	Multifamily (Mid-Rise) (N2-A, 6.97 acres)	72 DUs	297	20	28	Site Plan: 07-15-25
Proposed Zoning	Single-Family Attached (N2-A, 6.97 acres)	68 DUs	468	30	37	<i>Site Plan: 11-10-25</i>

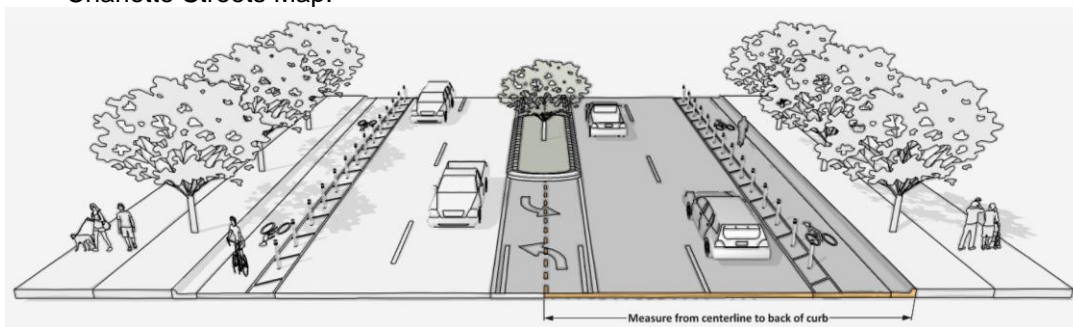
Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

Outstanding Issues

Strikethrough = Resolved

1. Curblines:

Steele Creek Road: The future location of the back of curb and gutter is minimum 38ft from roadway centerline to accommodate a 4+ Avenue, Buffered/Separated Bike Lanes per Charlotte Streets Map.

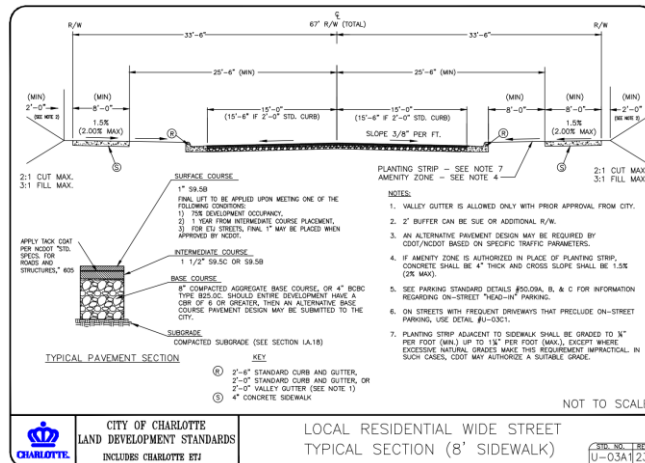


~~a. Limelight Lane: The future location of the back of curb and gutter is minimum 17.5' from roadway centerline following CLDSM U-03A1 detail.~~

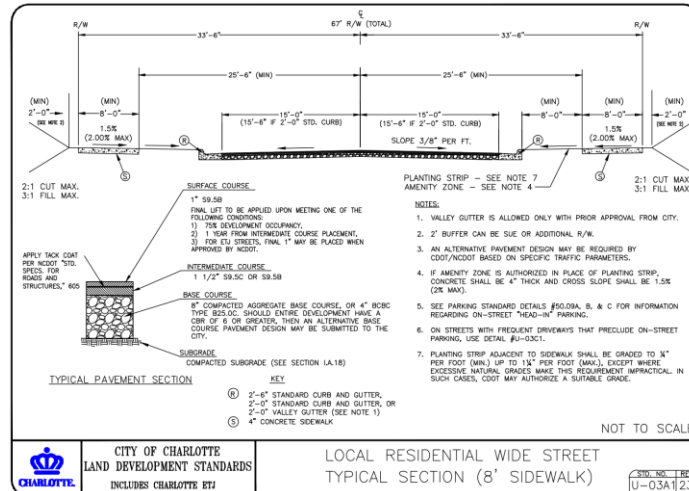
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b. ~~Red Spring Drive: The future location of the back of curb and gutter is minimum 17.5 feet from roadway centerline following CLDSM U-03A1 detail.~~



Label and dimension the curb and gutter from the centerline for each road on the site plan.

2. Comprehensive Transportation Review (CTR):

A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating site trips under ordinance thresholds found in the Charlotte Streets manual Table 3.1 and/or not triggering any other City TIS requirements.

- Revise site plan and conditional note(s) to commit to dedicate minimum 54-feet right-of-way from the road centerline along Steele Creek Road. The site plan shall label and dimension the right-of-way from the road centerline.

Clarifying Comment 11/20/2025: 54-feet is minimum Right-of-way dedication along Steele Creek Road. With addition of Right Turn Lane extend right-of-way dedication to the back of sidewalk and revise dimension(s) accordingly. If ROW is placed at the back of sidewalk and not 2 feet behind the sidewalk add a 2-foot sidewalk utility easement behind the sidewalk.

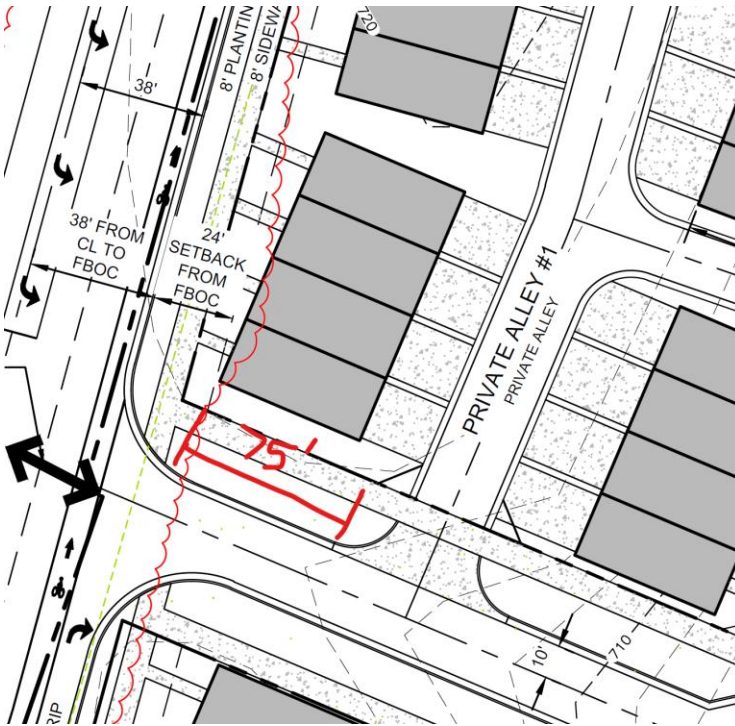
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4. ~~Revise site plan and conditional note(s) to commit to construct bicycle facilities by locating the curb and gutter a minimum 38 feet from the roadway centerline along Steele Creek Road.~~
5. ~~Revise site plan and conditional note(s) to commit to installing a right turn lane and left turn lane into the access on Steele Creek Road. Coordinate with NCDOT to determine storage lengths and taper lengths.~~
6. Revise site plan and conditional note(s) to provide a minimum 100-ft protected stem from Steele Creek Road measured from the Right-of-way.

Clarifying Comment 11/20/2025: If NCDOT is okay without a 100-ft protected stem provide correspondence from NCDOT to chris.manno@charlottenc.gov. If NCDOT does not have stem requirements the location of private alley #1 will still need to meet UDO requirements of 75 feet from an unsignalized intersection. Distance is measured from Point of Curvature (PC) of driveway to PC of intersection.

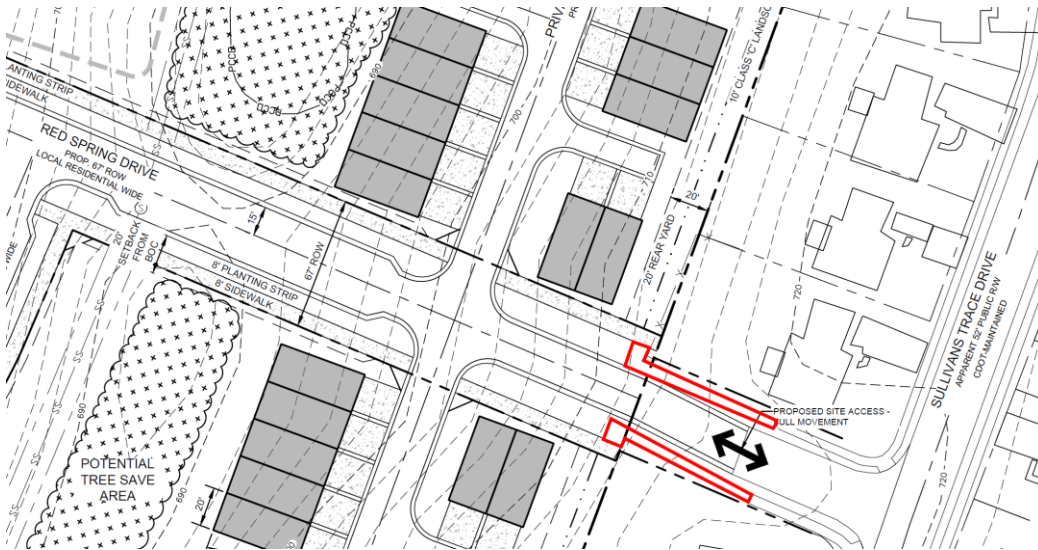


7. ~~Revise site plan and conditional note(s) to connect sidewalk to existing sidewalk along Steele Creek Road to the south.~~
8. ~~CDOT Request: Revise site plan and conditional note(s) to connect sidewalk along both sides of Red Spring Drive.~~

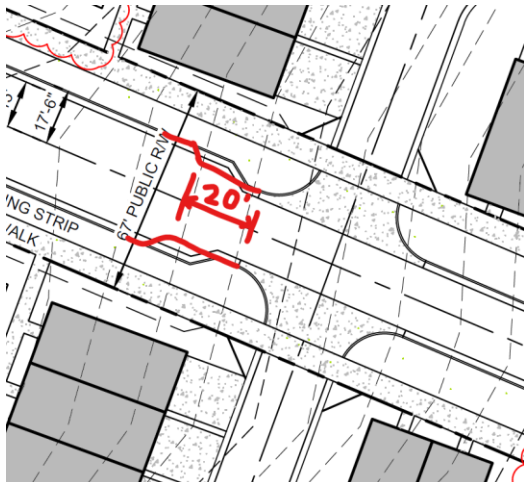
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9. ~~Modify conditional note 3.E to read “the Petitioner shall dedicate and convey in fee simple all rights-of-way to the City before the site’s first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2’ behind back of sidewalk where feasible.”~~
10. Add a conditional note specifying “All transportation improvements will be approved and constructed before the site’s first building certificate of occupancy is issued.”
11. Add conditional note specifying “All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or roadway projects taking place within the broad south west Mecklenburg area, by way of a private/public partnership effort or other public sector project support.”
12. **New Comment 11/20/2025:** Add 10x10 sight visibility triangle to driveway along Limelight Lane and Private Alley #3.
13. **New Comment 11/20/2025:** Ensure all on-street parking zones are a minimum of 20-ft (tangent) from the Point of Curvature of the driveway (Typical throughout the site plan).

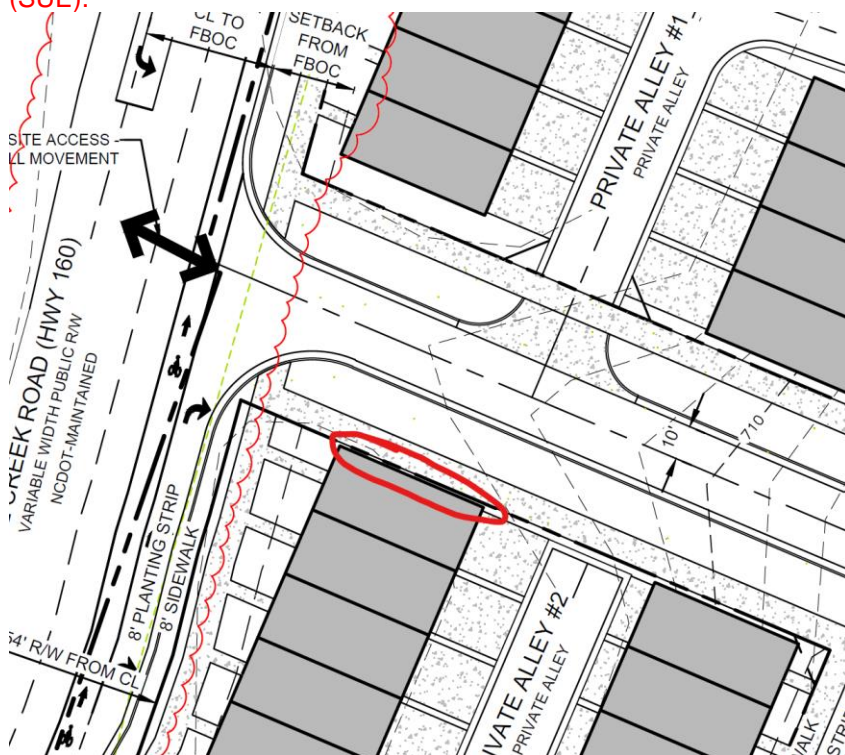


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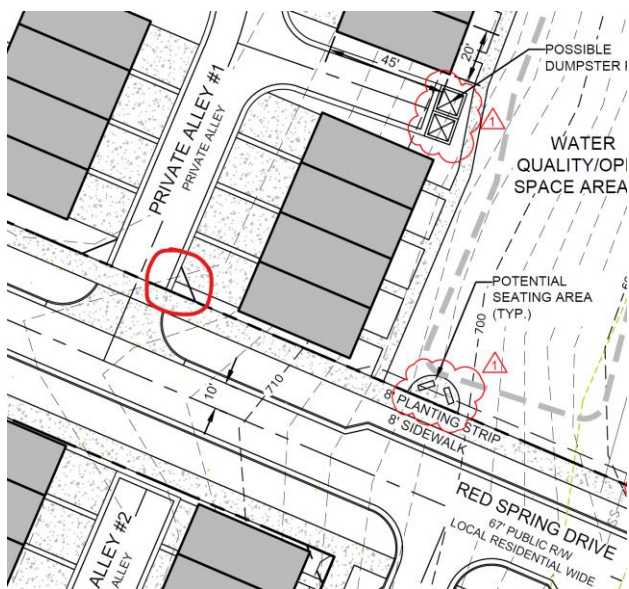
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14. **New Comment 11/20/2025:** Show a minimum 2ft ROW or SUE behind sidewalk throughout the site. Make sure no structures or seating areas are located within the Right-of-way or Sidewalk Utility Easement (SUE).



15. **New Comment 11/20/2025:** Revise site plan to eliminate conflict within 10x10 sight triangle. Car parked in driveway is in 10x10 sight triangle for first lot along private alley #1.

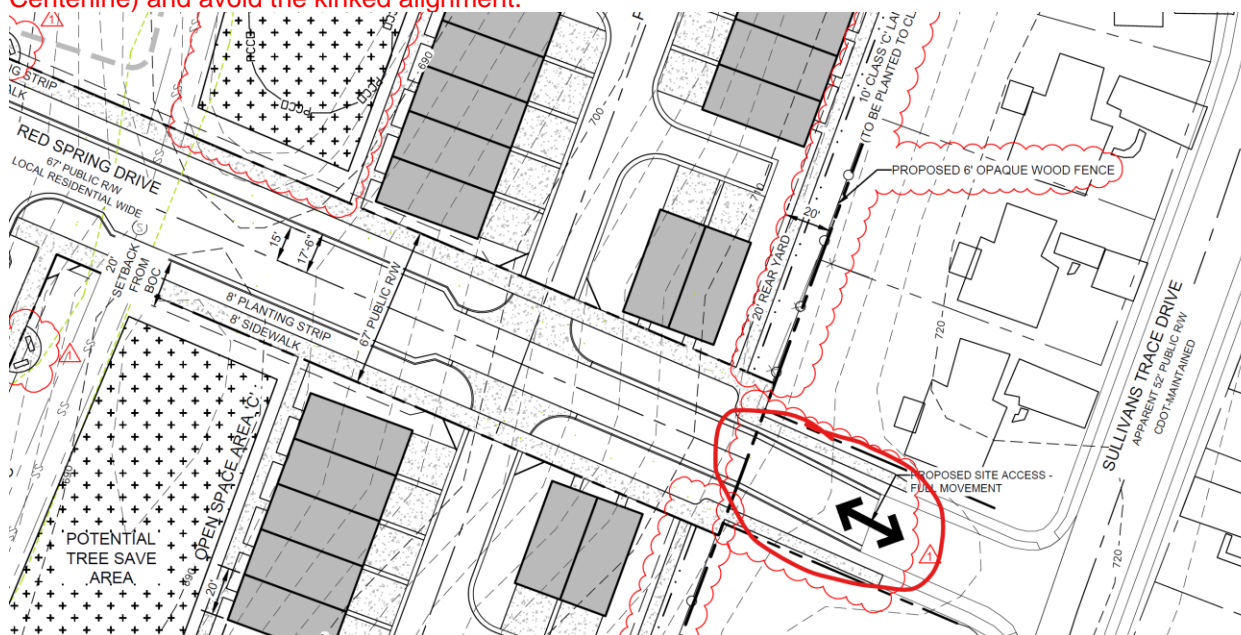


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16. **New Comment 11/20/2025:** Align Red Spring Drive with existing roadway (centered along ex. Centerline) and avoid the kinked alignment.



Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte Unified Development Ordinance.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and to comply with City of Charlotte Unified Development Ordinance regulations.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>