

# Rezoning Transportation Analysis

Petition Number: 2025-070

General Location Identifier: 03736123

**From: Jake Carpenter, PE**

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**Reviewer:**

**Isaiah Washington**

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## Revision Log:

Date	Description
8-28-2025	First Review (IW)
9-24-25	Second Review (IW)

## General Review Information

The petition is located adjacent to Beatties Ford Road, a State-maintained major arterial, north of Banner Ridge Boulevard, a privately maintained local street. The petition is located in a northwest wedge outside of Route 4, within the Westside Strategy Plan Study Area.

## Active Projects Near the Site:

- CIP - Beatties Ford Road/Sunset Road Pedestrian Improvements, Phase 1
  - Project ID: PM51216049
  - Location Description: Sunset Rd. from Day Lilly Ln. to Beatties Ford Rd.
  - Project Type: Pedestrian and Bike
  - Project Phase: Construction
  - Anticipated Construction Start Date: Mid 2016
  - Anticipated Completion Date: Late 2027
  - PM: Adam Jarman [adam.jarman@charlottenc.gov](mailto:adam.jarman@charlottenc.gov)

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CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

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Based on our review of the petition, we offer the following information for your consideration.

## Transportation Summary

The site is located adjacent to Beatties Ford Road, a State-maintained major arterial, north of Banner Ridge Boulevard, a privately maintained local street. A Comprehensive Transportation Review (CTR) is required for this site due to the site falling within the medium to high intensity development. Based on the 1,811 daily trips, this will trigger a Traffic Impact Study and Tier 2 multimodal assessment. Site plan and/or conditional note revisions are needed to commit to showing road extensions connecting to existing public roads University Church Drive and Lukes Drive, removing on street parking on the inside curve of University Church Drive, right turn lane on Beatties Ford per NCDOT, updating conditional notes and adding mitigations from Traffic Study once it is reviewed and approved. Further details are listed below.

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## Trip Generation

Scenario	Land Use	Intensity	Trip Generation			Source
			Daily Trips	AM Peak Hour Trips	PM Peak Hour Trips	
Existing Use	Vacant	-	-	-	-	<i>Tax Record</i>
Entitlement with Current Zoning	Single Family (N1-A, 30.37 acres)	91 Dwelling Units	925	68	91	<i>General Guidance from Planning</i>
<del>Proposed Zoning</del>	<del>Independent Living Facility</del> <del>Single-Family Attached</del> <del>Single-Family Detached</del> <del>(N2-B, 30.37 acres)</del>	<del>130 Dwelling Units</del> <del>95 Dwelling Units</del> <del>92 Dwelling Units</del>	<del>2,009</del>	<del>139</del>	<del>178</del>	<del>Site Plan: 06-15-25</del>
Proposed Zoning	Independent Living Facility Single-Family Attached Single-Family Detached (N2-B, 30.37 acres)	130 Dwelling Units 91 Dwelling Units 87 Dwelling Units	1,811	120	161	<i>Site Plan: 06-15-25</i>

**Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.**

### Outstanding Issues

**Strikethrough = Resolved**

#### 1. Curblin:

- a. ~~This site does not have frontage along existing public roads.~~

#### 2. Comprehensive Transportation Review (CTR):

A Traffic Impact Study is required for the complete review of this petition due to the site generating site trips over ordinance thresholds found in the Charlotte Streets Manual Table 3.1 and/or triggering other City TIS requirements.

Staff will receive the petition and begin review, but the public hearing will not be scheduled until the TIS is approved to allow for the minimum time necessary for CDOT to review the study and reach agreement with the petitioner on the required transportation commitments. Any agreed upon commitments shall be included on the last site plan submitted no fewer than 4 weeks prior to the public hearing. Therefore, additional traffic mitigation comments may be forthcoming once the TIS is approved by CDOT (and NCDOT).

The petition's zoning falls within the medium to high intensity development which based on the 1,811 daily trips will trigger Tier 2 (6 mitigation points) for multimodal assessment. The petitioner shall review and assess the publicly accessible pedestrian network within ¼ mile walking distance of the site to identify multimodal infrastructure to meet the Tier 2 (6 mitigation points) for multimodal assessment.

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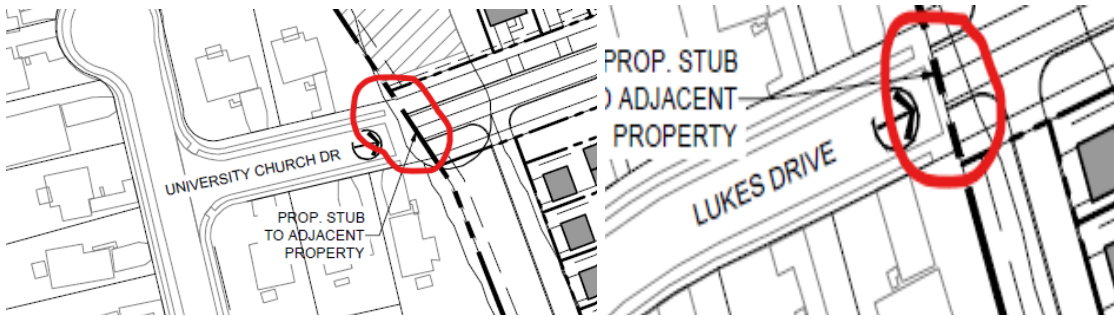
Complete CTR summary table and include on the revised plan. CTR table link (use Site Data Table – CTR Summary tab): <https://www.charlottenc.gov/files/sharedassets/city/v/2/growth-and-development/documents/dev-center-fees/resources/udo-site-data-tables-curb-ramps-and-ctr.xlsx>

**New Comment 9.24.2025:** See comment above TIS still outstanding.

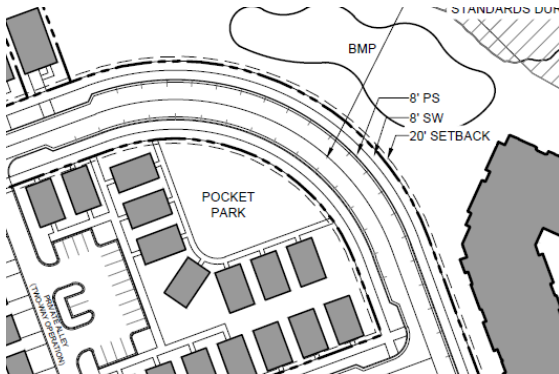
3. **Outstanding Comment from 8/28/25:** Add a conditional note specifying “A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained Street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.”

**Clarifying Comment:** This is a standard comment that will need to be added to the conditional notes.

4. **Outstanding Comment from 8/28/25:** Revise site plan and conditional note(s) to commit to show the extension and connection between proposed roads and existing roads. Site plan shall callout the proposed roadway stubs stating that the proposed stub shall connect with the adjacent existing roads. Add a conditional note also committing to this improvement.



5. **Outstanding Comment from 8/28/25:** Revise site plan and conditional note(s) to commit to removing on street parking from the inside curve of University Church Drive.



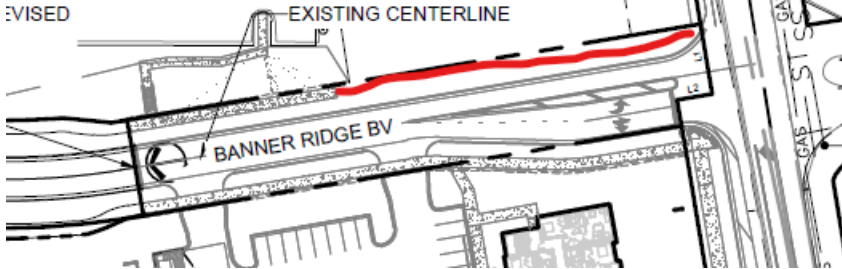
6. **Outstanding Comment from 8/28/25:** Revise site plan and conditional note(s) to commit to adding conditional note to meet all minimum roadway requirements including but not limited to vertical and horizontal roadway alignment and intersection angle requirements. (i.e. appears to be a reverse curve over Banner Ridge, would need to verify that it meets the minimum requirements and the proposed southwest intersection internal to the site must have a minimum intersection angle of 75 degrees)

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7. **Outstanding Comment from 8/28/25: NCDOT REQUIREMENT:** Revise site plan and conditional note(s) to commit to a 50' right turn lane on Beatties Ford Road in coordination with NCDOT. Add a conditional note committing to this improvement and further coordination with CDOT and NCDOT during permitting. Site plan shall dimension, and label right turn lane from the roadway centerline.
8. **Outstanding Comment from 8/28/25:** Revise site plan and conditional note(s) to commit to extending the sidewalk on Banner Ridge Blvd to Beatties Ford Road.



9. **Outstanding Comment from 8/28/25:** Revise site plan and conditional note(s) by adding pedestrian ramps to the intersections within the proposed development. You may need to adjust some of the on street parking stalls to accommodate.



10. **Outstanding Comment from 8/28/25:** Add a conditional note specifying “the Petitioner shall dedicate and convey in fee simple all rights-of-way to the City before the site’s first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2’ behind back of sidewalk where feasible.”
11. **Outstanding Comment from 8/28/25:** Add a conditional note specifying “All transportation improvements will be approved and constructed before the site’s first building certificate of occupancy is issued.”

The petitioner may phase transportation improvements if said improvements and phasing are approved by CDOT and explicitly described in site plan notes.

12. **Outstanding Comment from 8/28/25:** Add conditional note specifying “All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development

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or roadway projects taking place within the broad north Western Mecklenburg area, by way of a private/public partnership effort or other public sector project support."

## Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte Unified Development Ordinance.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and to comply with City of Charlotte Unified Development Ordinance regulations.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>