

Rezoning Transportation Analysis

Petition Number: 2025-031

General Location Identifier: 04711202, 04711206

From: Jake Carpenter, PE

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Revision Log:

Date	Description
04-23-25	First Review (TM)
05-21-25	Second Review (TM)
10-23-25	Third Review (TM)

General Review Information

The petition is located at the intersection of Neal Road, a City-maintained arterial, and IBM Drive, a City-maintained arterial. The petition is located in a northeast wedge inside of Route 4

Active Projects Near the Site:

- N/A

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located at the corner of Neal Road, a city maintained arterial, and IBM Drive, a city maintained arterial. A Comprehensive Transportation Review (CTR) is required for this site due to the site falling within the low intensity development. Based on the 2,005 daily trips, this will trigger a Traffic Impact Study and Tier 3 multimodal assessment. TIS was approved week of 10/20/2025. Site plan and/or conditional note revisions are needed to commit to dimension the back of curb location along Catalyst Drive. CDOT also request that additional ROW is provided along Neal Road to provide space for a Right Turn Lane to be installed. Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation			Source
			Daily Trips	AM Peak Hour Trips	PM Peak Hour Trips	
Existing Use	Vacant	-	-	-	-	<i>Tax Record</i>
Entitlement with Current Zoning	Townhomes, Duplexes, and Triplexes (R8-MF, 23.369 acres)	157 Dwelling Units	1,146	76	90	<i>General Guidance from Planning & RZ# 2020-102</i>
Proposed Zoning	Multifamily Attached Multifamily Stacked (N2-B, 23.369 acres)	73 Dwelling Units 324 Dwelling Units	2,658	155	200	<i>Site Plan: 03-13-25</i>
Proposed Zoning	Multifamily Attached Multifamily Stacked (N2-B, 23.369 acres)	73 Dwelling Units 324 Dwelling Units	2,005	163	167	<i>Site Plan: 10-13-25</i>

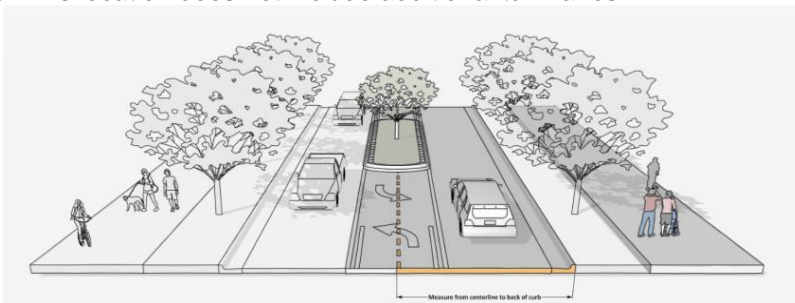
Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

Outstanding Issues

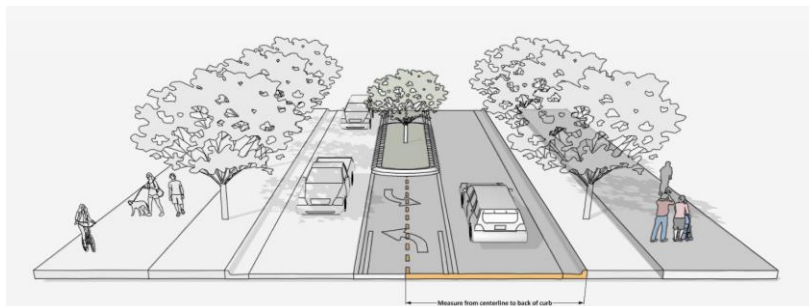
Strikethrough = Resolved

1. Curblines:

- a. ~~IBM Drive:~~ Location of future curb and gutter is to be 19 feet from roadway centerline to back of curb. This location does not include additional turn lanes.



- b. ~~Neal Road:~~ Location of future curb and gutter is to be 19 feet from roadway centerline to back of curb. This location does not include additional turn lanes.

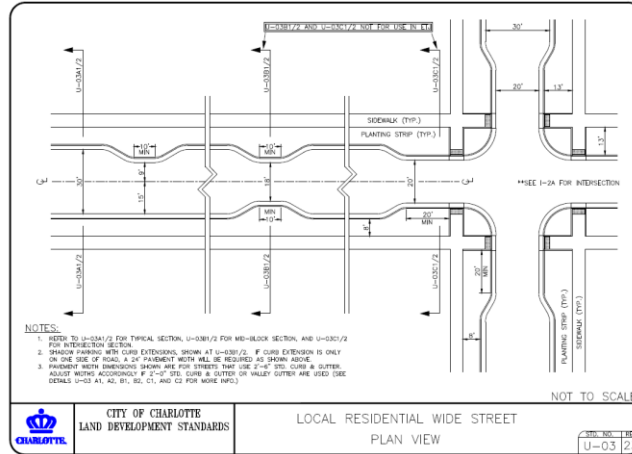


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- c. **Catalyst Drive:** Location of future curb and gutter is to follow CLDSM U-03.



Outstanding Comment 4.23.2025: Label and dimension the curb and gutter from the centerline for each road on the site plan.

2. ~~Comprehensive Transportation Review (CTR):~~

A Traffic Impact Study is required for the complete review of this petition due to the site generating site trips over ordinance thresholds found in the Charlotte Streets Manual Table 3.1 and/or triggering other City TIS requirements.

~~Staff will receive the petition and begin review, but the public hearing will not be scheduled until the TIS is approved to allow for the minimum time necessary for CDOT to review the study and reach agreement with the petitioner on the required transportation commitments. Any agreed upon commitments shall be included on the last site plan submitted no fewer than 4 weeks prior to the public hearing.~~ Therefore, additional traffic mitigation comments may be forthcoming once the TIS is approved by CDOT (and NCDOT).

The petition's zoning falls within the low intensity development which based on the 2,005 daily trips will trigger Tier 3 (9 mitigation points) for multimodal assessment. The petitioner shall review and assess the publicly accessible pedestrian network within ¼ mile walking distance of the site to identify multimodal infrastructure to meet the Tier 3 (9 mitigation points) for multimodal assessment.

Outstanding Comment 5.19.2025: TIS is still outstanding, and a scope has not been submitted.

3. **Outstanding Comment 5.19.2025:** ~~Revise site plan and conditional note(s) to commit to dedicate 39-foot (Back of Shared Use path) of right of way from the road centerline along both Neal Road and IBM Drive. The site plan shall label and dimension the right-of-way from the road centerline.~~
4. **Outstanding Comment 5.19.2025:** ~~Revise site plan and conditional note(s) to commit to dedicate 31.5-foot (Back of sidewalk) of right-of-way from the road centerline along Catalyst Road. The site plan shall label and dimension the right-of-way from the road centerline.~~

Rezoning Transportation Analysis

Petition Number: 2025-031

General Location Identifier: 04711202, 04711206

5. ~~Outstanding Comment 5-19-2025:~~ Revise site plan and conditional note(s) to commit to revising site plan to match previously approved street network from rezoning 2020-102 including the access point on IBM in coordination with Subdivision. Site plan shall label and dimension each road to meet CLDSM standard detail U-03.
 6. ~~Outstanding Comment from 04-23-25:~~ Revise site plan and conditional note(s) to add sidewalk on all internal street networks. Sidewalk to be a min of 8 feet wide per UDO table 33-4. Site plan shall label, and dimension from the roadway centerline.
 7. ~~Outstanding Comment from 04-23-25:~~ Revise site plan and conditional note(s) to add sidewalk along Catalyst Drive meeting UDO requirement of 8-foot wide and an 8-foot planting strip. Site plan shall label, and dimension each from the roadway centerline.
 8. ~~Outstanding Comment from 04-23-25:~~ Revise site plan and conditional note(s) to commit to construct bicycle facilities by installing a 12' multi-use path in accordance with the council-adopted Charlotte Streets Map along Neal Road and IBM Drive frontages. Multi-use paths maintained by the City shall meet ADA/PROWAG requirements.
- Clarifying Comment 05-21-25:** Add a conditional note committing to the construction and placement of the SUP along Neal Road and IBM Drive frontages.
9. ~~Add a conditional note specifying "A Right of Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained Street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements."~~
 10. ~~Add a conditional note specifying "the Petitioner shall dedicate and convey in fee simple all rights-of-way to the City before the site's first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible."~~
 11. **New CDOT REQUEST 10-23-25:** Add conditional note to provide a minimum of 50' feet of ROW from the centerline of IBM Drive to accommodate the future right turn lane. Some of the ROW can be placed in an SUE. This ROW will allow for a right turn lane to be constructed at the signal which will drastically increase the functionality of the intersection.

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Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte Unified Development Ordinance.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and to comply with City of Charlotte Unified Development Ordinance regulations.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>