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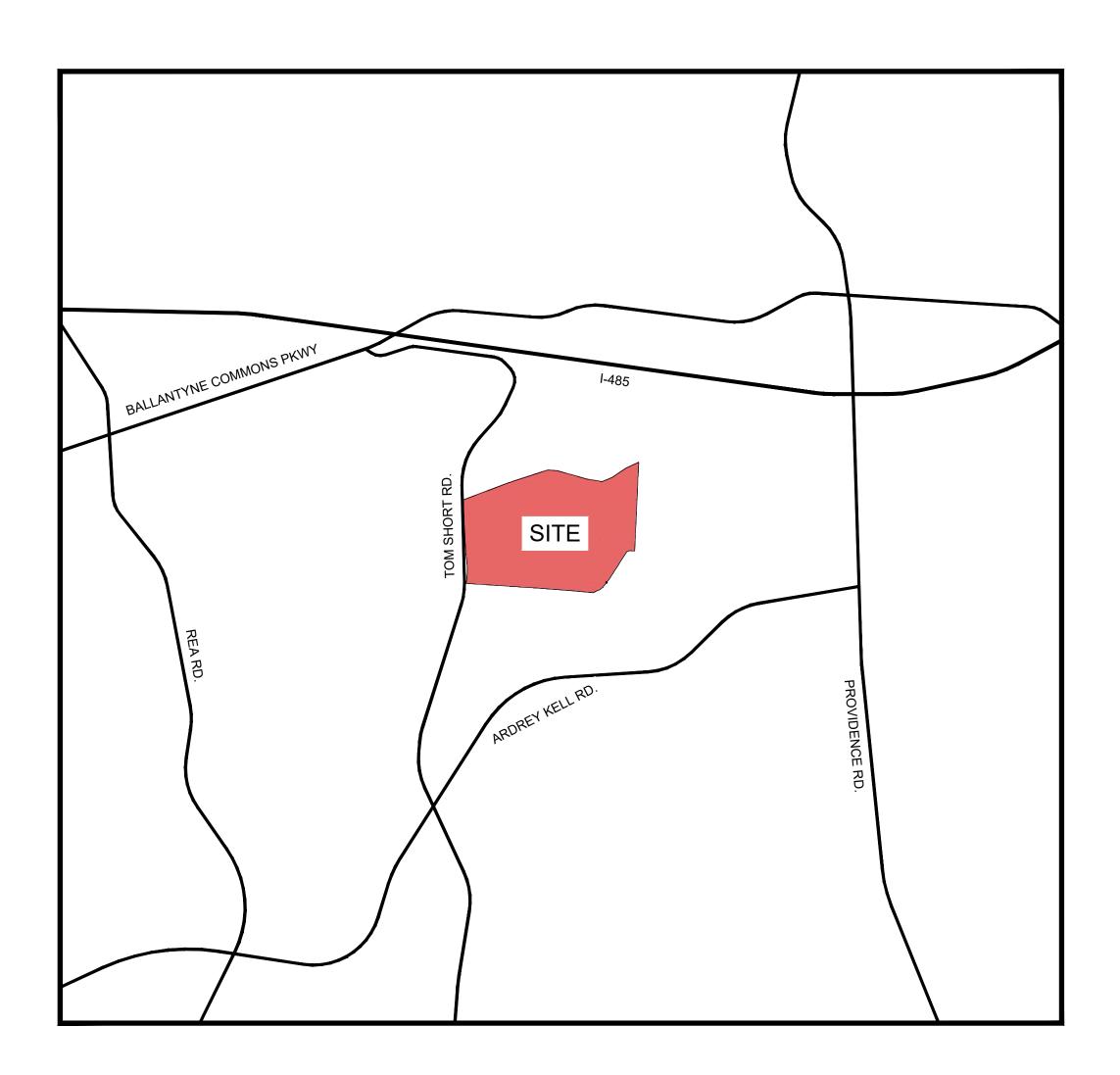
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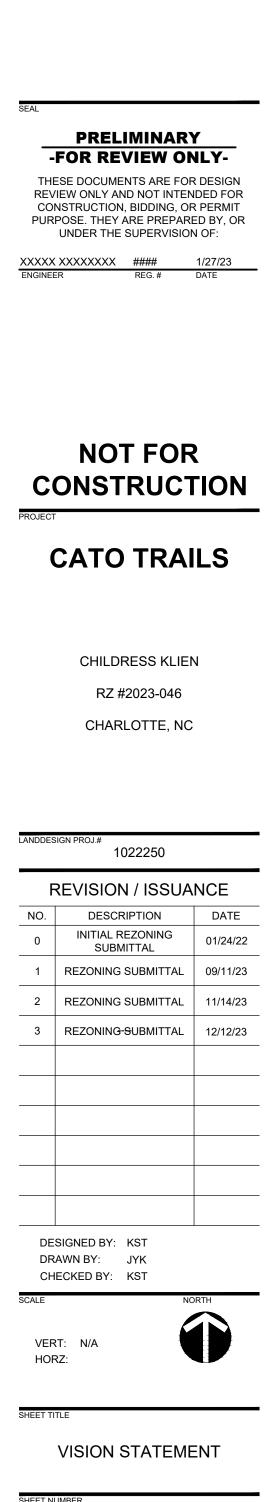
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Rezoning of the Cato Farms property will create a master planned residential community that will be integrated into the mixed residential community that has developed south of I-485 and east and west of Providence Rd. Cato Farms planned community will celebrate its natural features, preservation areas and open space with the development of an incredibly robust walking trail system both within the approximately 13-acre natural preserve area and throughout the Site as a principal organizing element for a planned CMS Middle school and a mixed of residential uses. This walking trail system will not only enhance the accessibility within the community but also create opportunities for active lifestyle, pedestrian, and bike access from other nearby existing residential communities and from nearby school, park, and greenway facilities. As a further embrace of the Cato Farms history, the pastoral theme will be on display with possible features such as split rail fences, community amenity buildings with rural designs, and the like.



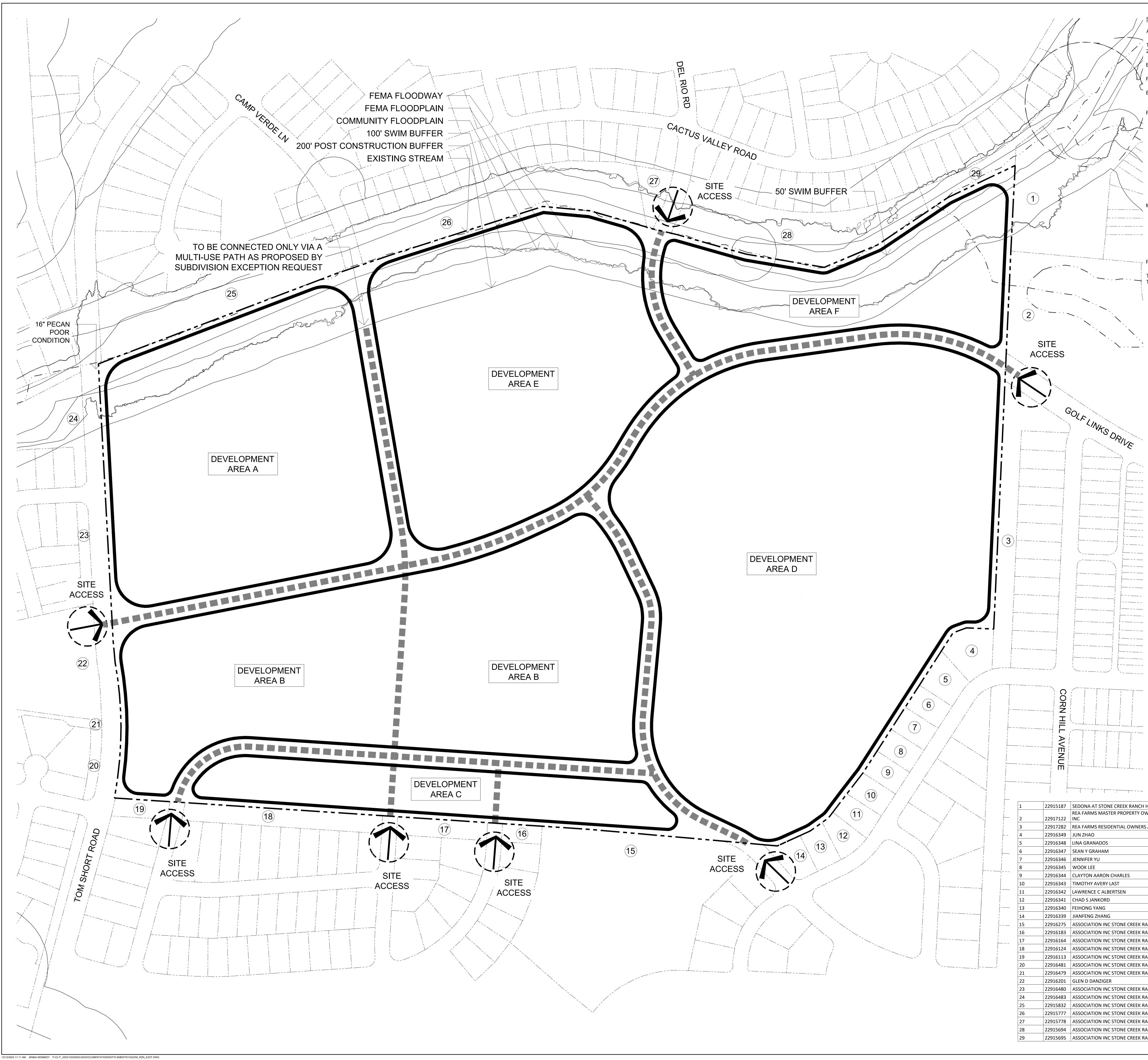
VISION STATEMENT:



LandDesign.

223 NORTH GRAHAM STREE CHARLOTTE, NC 28202 704.333.0325 WWW.LANDDESIGN.COM







ACREAGE: ± 124.916 AC

TAX PARCEL #S: 229-161-01, 229-161-03, 229-161-05, AND 229-161-84

EXISTING ZONING: MX-1 (INNOVATIVE)

PROPOSED ZONING: MX-2 (INNOVATIVE)

EXISTING USES: THREE SINGLE-FAMILY HOMES AND RELATED STRUCTURES

PROPOSED USES: A SECONDARY SCHOOL CONSISTING OF CLASSROOMS, CAFETERIA, ACTIVITY ROOMS AND ASSOCIATED USES, AND UP TO 917 RESIDENTIAL DWELLING UNITS MADE UP OF A MIX OF RESIDENTIAL UNIT TYPES AS DESCRIBED BELOW, THAT MAY INCLUDE UP TO 682 MULTI-FAMILY RESIDENTIAL DWELLING UNITS, 211 ATTACHED DWELLING UNITS, 24 SINGLE-FAMILY DETACHED RESIDENTIAL DWELLING UNITS, TOGETHER WITH ALL ACCESSORY USES PERMITTED WITHIN THE MX-2 INNOVATIVE ZONING DISTRICT, SUBJECT TO THE RIGHT TO ALTER THE NUMBER OF RESIDENTIAL UNITS LOCATED WITHIN DEVELOPMENT AREAS AND OTHERWISE AS MORE PARTICULARLY DESCRIBED IN SECTION 3 BELOW.

MAXIMUM BUILDING HEIGHT: THE FOLLOWING MAXIMUM BUILDING HEIGHTS SHALL APPLY TO THE DEVELOPMENT AREAS (BUILDING HEIGHT WILL BE MEASURED AS DEFINED BY THE ORDINANCE): DEVELOPMENT AREA A: [60']; DEVELOPMENT AREA B: [65']; DEVELOPMENT AREA C: [40']; DEVELOPMENT AREA D: [50']; DEVELOPMENT AREA E: [65']; AND DEVELOPMENT AREA F: [50'].

PARKING: AS REQUIRED BY THE ORDINANCE.

TREE SAVE: AS REQUIRED BY THE ORDINANCE - 15% OF TOTAL ACREAGE TO BE PROVIDED. REQUIRED: 15% X 124.916 AC = 18.74 AC

_ ____

SITE LEGEND PROPOSED PUBLIC ROAD NETWORK

DEVELOPMENT AREA

PROPOSED SITE ACCESS



30498-930	MX-1(INNOV)	COMMON AREA
32375-181	MUDD-O	COMMON AREA
34173-249	MUDD-O	COMMON AREA
31848-695	MX-1(INNOV)	SINGLE FAMILY
34791-653	MX-1(INNOV)	SINGLE FAMILY
22848-51	MX-1(INNOV)	SINGLE FAMILY
34073-913	MX-1(INNOV)	SINGLE FAMILY
22737-103	MX-1(INNOV)	SINGLE FAMILY
37836-992	MX-1(INNOV)	SINGLE FAMILY
32847-683	MX-1(INNOV)	SINGLE FAMILY
25443-164	MX-1(INNOV)	SINGLE FAMILY
35864-411	MX-1(INNOV)	SINGLE FAMILY
29134-604	MX-1(INNOV)	SINGLE FAMILY
27491-348	MX-1(INNOV)	SINGLE FAMILY
26813-191	MX-1(INNOV)	COMMON AREA
26813-188	R-3	COMMON AREA
26813-188	R-3	COMMON AREA
32121-468	R-3	SINGLE FAMILY
26813-188	R-3	COMMON AREA
26813-188	R-3	COMMON AREA
27040-935	MX-1(INNOV)	COMMON AREA
	32375-181 34173-249 31848-695 34791-653 22848-51 34073-913 22737-103 37836-992 32847-683 25443-164 35864-411 29134-604 27491-348 26813-191 26813-191 26813-191 26813-193 26813-194 26813-195 26813-188 32121-468 26813-188 26813-188 26813-188 26813-188 27040-935 27040-935 27040-935	32375-181 MUDD-O 34173-249 MUDD-O 31848-695 MX-1(INNOV) 34791-653 MX-1(INNOV) 22848-51 MX-1(INNOV) 34073-913 MX-1(INNOV) 34073-913 MX-1(INNOV) 32847-683 MX-1(INNOV) 32847-683 MX-1(INNOV) 32847-683 MX-1(INNOV) 35864-411 MX-1(INNOV) 29134-604 MX-1(INNOV) 26813-191 MX-1(INNOV) 26813-188 R-3 27040-935 MX-1(INNOV)

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CATO TRAILS

CHILDRESS KLIEN RZ #2023-046 CHARLOTTE, NC

	LANDDESIGN PROJ.# 1022250		
F	REVISION / ISSUANCE		
NO.	DESCRIPTION	DATE	
0	INITIAL REZONING SUBMITTAL	01/24/22	
1	REZONING SUBMITTAL	09/11/23	
2	REZONING SUBMITTAL	11/14/23	
3	REZONING- S UBMITTAL	12/12/23	
DESIGNED BY: KST DRAWN BY: JYK			
DRAWN BY: JYK CHECKED BY: KST			
SCALE NORTH			
VERT: N/A HORZ: 1" = 100'			
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TECHNICAL DATA			

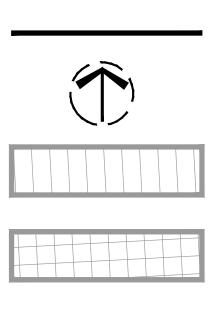
RZ-1



EGEND	
D PUBLIC WORK	
IENT AREA	
D SITE ACCESS	
L PASSIVE OPEN	

POTENTIAL ACTIVE OPEN SPACE

POTENTIAL TRAIL NETWORK ---------



ORIGINAL SHEET SIZE: 36" X 48"

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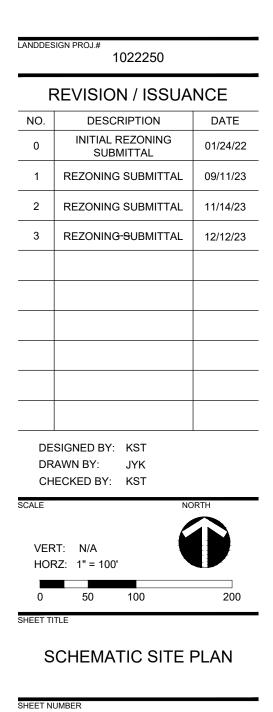
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 1/25/23

 ENGINEER
 REG. #
 DATE

NOT FOR CONSTRUCTION

CATO TRAILS

CHILDRESS KLIEN RZ #2023-046 CHARLOTTE, NC



RZ-2

Childress Klein Properties & Charlotte-Mecklenburg Schools - Tom Short Rd			
Development Standards			
12/12/23			

Rezoning Petition No. 2023-046

Site Development Data:

--Acreage: ± 124.602 acres --Tax Parcel #s: 229-161-01, 229-161-03, 229-161-05, and 229-161-84

--Existing Zoning: MX-1 (Innovative) --Proposed Zoning: MX-2 (Innovative) --Existing Uses: Three single-family homes and related structures

--Proposed Uses: A secondary school consisting of classrooms including mobile class rooms, cafeteria, activity rooms and associated uses, and up to 917 residential dwelling units made up of a mix of residential unit types as described AI below, that may include up to 682 multi-family residential dwelling units, 211 attached dwelling units, 24 he single-family detached residential dwelling units, together with all accessory uses permitted within the MX-2 pInnovative zoning district, subject to the right to alter the number of residential units located within Development Areas and otherwise as more particularly described in Section 3 below. --Maximum Building Height: The following maximum building heights shall apply to the Development Areas (building height will be measured as defined by the Ordinance): Development Area A: +60'; Development Area B: 65';

Development Area C: 40'; Development Area D: 50'; Development Area E: 65'; and Development Area F: 50'. --Parking: As required by the Ordinance.

1. General Provisions:

a. Site Location. These Development Standards, the Technical Data Sheet, Schematic Site Plan, and related graphics form the rezoning plan (collectively referred to as the "Rezoning Plan") associated with the Rezoning Petition filed by -I Children Klein Properties & Charlotte-Mecklenburg Schools together (the "Petitioner") to accommodate development of a or s residential community and a secondary school on the approximately 124.602-acre site located on Tom Short Road (the "Site"). b. **Zoning Districts/Ordinance**. Development of the Site will be governed by the Rezoning Plan as well as the applicable or provisions of the City of Charlotte Zoning Ordinance in existence as of the filing of the Rezoning (the "Ordinance"), not the Uniform Development Ordinance. Unless the Rezoning Plan establishes more stringent standards, the regulations established under the Ordinance for the MX-2 (Innovative) zoning classification shall govern all development taking place on the Site, ^a subject to the Innovative Provisions provided below.

c. **Development Areas.** For ease of reference and as an organizing principal associated with the master planned community, the Rezoning Plan sets forth five (5) general development areas (and other sub-areas within the Development *P* Areas) as generally depicted on the Technical Data Sheet as Development Areas A, B, C, D, E and F (each a "Development Area" and collectively the "Development Areas"). Graphics and Alterations. The schematic depictions of the uses, parking areas, sidewalks, structures and buildings, building elevations, driveways, streets, Development Areas (as defined below), open space areas and other development matters and site elements (collectively the "Development/Site Elements") set forth on the Rezoning Plan shall be reviewed in conjunction with the provisions of these Development Standards. The layout, locations, sizes, and formulations of the Development/Site Elements depicted on the Rezoning Plan are graphic representations of the Development/Site elements proposed. Changes to the Rezoning Plan not anticipated by the Rezoning Plan will be reviewed and approved as allowed by Section 6.207 of the Ordinance. Since the project has not undergone the design development and construction phases, it is intended that this Rezoning Plan provide for flexibility in allowing some alterations or modifications from the graphic representations of the Development/Site

Elements. Therefore, there may be instances where minor modifications will be allowed without requiring the Administrative Amendment Process per Section 6.207 of the Ordinance. These instances would include changes to graphics if they are: i. expressly permitted by the Rezoning Plan (it is understood that if a modification is expressly permitted by the

Rezoning Plan it is deemed a minor modification for the purposes of these Development Standards); or ii. minor and don't materially change the overall design intent depicted on the Rezoning Plan; or ii. modifications to move structures graphically depicted on the Rezoning Plan closer to adjacent properties in a residential district or abutting residential use but no closer than the "external building line" (in this case the external setbacks, rear yards, or buffer areas) indicated on the Rezoning Plan.

The Planning Director will determine if such minor modifications are allowed per this amended process, and if it is determined • Convert the existing eastbound thru-left turn lane on the to a combined left-thru-right turn lane to allow an additional that the alteration does not meet the criteria described above, the Petitioner shall then follow the Administrative Amendment Process per Section 6.207 of the Ordinance; in each instance, however, subject to the Petitioner's appeal rights set forth in the Ordinance.

Number of Buildings Principal and Accessory. Notwithstanding the number of buildings or lots shown on the Rezoning Plan, the number of principle use buildings constructed on the Site shall not exceed 120 (excluding accessory use buildings and mobile classroom buildings). f. Planned/Unified Development. The Site shall be viewed as a planned/unified development plan as to the Development/Site Elements, Development Areas and portions of the Site generally depicted on the Rezoning Plan. As such,

side and rear yards, buffers, building height separation standards, public/private street frontage requirements, and other similar zoning/subdivision standards will not be required internally between improvements and other Development/Site Elements located on the Site. Furthermore, the Petitioner and/or owners of the applicable portion of the Site under development reserve the right to subdivide the portions or all of the Site and create lots within the interior of the Site without regard to any such internal separation standards, and public/private street frontage requirements, provided, however, all such separation standards Phase 3 Conditions along the exterior boundary of the Site shall be adhered to as a whole and not individual portions, Development Areas or lots located therein.

g. Gross Floor Area Clarification. When determining the maximum development levels set forth in this Rezoning Plan, 4. Providence Road & Ardrey Kell Road (Signalized) gross floor area as defined in the Ordinance shall exclude any -surface or structured parking facilities (including, without limitation, corridors, and elevators within such facilities), and outdoor dining and gathering areas whether on the roof of the building or at street level. h. Project Vision Overall Design Intent

Cato Farms will be a master planned residential community that will be integrated into the mixed residential community that has developed south of I-485 and east and west of Providence Rd. Cato Farms planned community will celebrate its natural features, preservation areas and open space with the development of an incredibly robust walking trail system both within the *Phase 3 Conditions* approximately 12-acre natural preserve area and throughout the Site as a principal organizing element for a planned CMS Middle school and a mixed of residential uses. This walking trail system will not only enhance the accessibility within the • No additional suggested improvements. community but also create opportunities for active lifestyle, pedestrian, and bike access from other nearby existing residential communities and from nearby school, park, and greenway facilities. As a further embrace of the Cato Farms history, the pastoral theme will be on display with possible features such as split rail fences, community amenity buildings with rural designs, and the like.

2. Lot Standards and Innovative Provisions for MX-2 (Innovative) Area:

a. **Single-Family Detached.** The Petitioner hereby seeks the following Innovative Development Standards in connection with single-family detached development taking place within Development Area C and any such single family detached development that might take place in other Development Areas B, D and E to accommodate a variety of setback and yard widths and other development elements so as to allow for a pedestrian friendly residential community. It is understood that certain provisions listed below are standard ordinance requirements and are included within this section for ease of review.

- i. A minimum lot size for single-family detached lots of 4,500 square feet.
- ii. A minimum lot width for single-family detached lots of forty (40') feet.

iii. A minimum front setback for single-family detached lots of 16-feet as measured from the proposed back of curb; in the event a driveway is provided without a garage, the setback shall be increased to twenty (20') feet as measured from the back of the sidewalk. If a garage is provided, the garage face will comply with the Chapter 12 of the *Phase 3 Conditions* Ordinance

iv.A minimum rear yard for single-family detached lots of twenty (20') feet unless lot has alley access in which a rear yard of five (5') feet is allowed;

v. A minimum side yard of five (5') feet for single family detached lots (including along public streets);

common open space;

vi. Private alleys shall follow CLDSM standards; and

vii. Maximum building coverage for single family detached shall be seventy (70%) percent.

- Attached Dwelling Units. The Petitioner hereby seeks the following Innovative Development Standards in connection with attached dwelling unit development taking place within Development Areas B, D and E to accommodate a variety of setback and yard widths and other development elements so as to allow for a pedestrian friendly residential community:
- i. Petitioner shall provide a minimum of a 16-foot setback as measured from the proposed back of curb. Stoops and stairs may encroach three (3) feet into the setback as a "transition zone;"

ii. For alley loaded attached dwelling units, driveway lengths shall be a minimum of 5-7' or 20' and greater measured from the back of curb to the face of garage;

iii. A minimum rear yard for attached dwelling units of twenty (20') feet unless lot has alley access in which a rear yard of five (5') feet is allowed;

iv.A minimum side yard of five (5') feet for attached dwelling units (including along public streets);

v. Private allevs shall follow CLDSM standards; and

acres.

vi. Attached dwelling units may front on public or private streets (if private streets are used, they will not be gated) or common open space.

c. In addition, the Petitioner reserves the right to modify the Innovative Provisions described above or seek other innovative development standards in the future pursuant to the applicable process set forth in the Ordinance.

Permitted Uses, Development Area Limitations, and Transfer & Conversion Rights:

a. The Site may be developed with up to 917 residential dwelling units that may include up to 682 multi-family residential units, 211 attached dwelling units, 24 single-family detached residential units, and a secondary school 12. Ballantyne Commons Parkway & Tom Short Road (Signalized) consisting of classrooms including mobile classrooms, cafeteria, activity rooms and other associated uses, in accordance with the Innovative Provisions described above; each such use as allowed by right and under prescribed *Phase 2 Conditions* conditions, together with accessory uses as permitted in the MX-2 Innovative zoning district, including, without limitation, community clubhouse, recreation, open space and related uses (e.g. improved passive and active open spaces, gathering shelters, gazebos, ball fields, maintenance buildings, outdoor recreational uses, and other uses typically associated with residential communities).

NOTE: the locations and number of residential units allocated to individual Development Areas as described below and within the Development Areas on the Rezoning Plan may be increased by the number listed below as the "Adjusted Increase Amount" provided that other unit amounts are reduced as described below and in any event the total number of multi-family units on the Site does not exceed 682 units, and the total number of all dwelling units on the Site does not exceed 917 units. **13.** Tom Short Road & Access "A"/Golf Links Drive (Future Signal)

b. Density Bonus. To accommodate the proposed CMS school on the Site the Petitioner requests a density bonus as We propose the following access configuration: allowed by the Ordinance by increasing the amount of required Common Open Space from 10% to 19.7% (24.54 increase of 81 units). The proposed Common Open space may be located throughout the Site.

c. Development Area A may be developed as a secondary school consisting of classrooms including mobile classrooms, cafeteria, activity rooms and associated uses, together with all accessory uses permitted within the MX-2 Innovative zoning district, as more particularly described in these Development Standards.

d. Development Area D & E may be developed with (i) up to 682 multi-family and/or attached dwelling units and associated uses such as clubhouse and amenity areas together with all accessory uses permitted within the MX-2 Innovative zoning district, provided, however the number of permitted units may be increased for Development Area D & E by up to 68 units by way of a reduction in the number of residential units allocated in the aggregate to Development Area B up provided Internal Intersection "A" that the total of multi-family units does not exceed 682 multi-family units; and (ii) a ± 12 acre natural preserve area that will contain trails and other natural amenities.

e. <u>Development Area C</u> may be developed with up to 24 single family detached homes/lots together with all accessory synchro files): uses permitted within the MX-2 Innovative zoning district, provided, however the number of single family detached homes may be increased for Development Area C by up to 3 homes/lots by way of a reduction in the number of • Install a traffic signal. residential units allocated in the aggregate to Development Areas B, D and E.

f. Development Area B may be developed with (i) up to 210 attached dwelling units or single family detached homes/lots together with all accessory uses permitted within the MX-2 Innovative zoning district, provided, however the number of • Northbound - A combined left-thru-right turn lane. attached dwelling units or single family detached homes/lots may be increased for Development Area B by up to (21) units/homes by way of a reduction in the number of residential units/homes allocated in the aggregate to Development Areas C, D and E; and (ii) open space/community amenities which shall include an improve active open space area(s) with $[\pm 4]$

g. <u>Development Area F</u> may be developed with open space, storm water structures and trails and other associated open space amenities.

h. No less than 125 attached dwelling units and no less than 18 single-family detached homes/lots will be constructed on

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the Site with such attached dwelling units a of occupancy for the 600 th multi-family un
Transportation Improvements, Access an
Proposed Improvements.
nprovements Generally; Phases of Development opperation with other parties who may implement cordance with the following implementation pro- ay be amended to align with final approved Trans
Il transportation improvements will be substant efore the first building certificate of occupancy i <i>hase 1, 2 or 3 Development</i> as described below onditions set forth in subsection a. below (subject ad as provided in the remaining provisions of this
OTE: FOLLOWING TRANSPORTATION HANGE BASED FURTHER REVIEW RANSPORTATION IMPACT STUDY COMP
Phase 1 Development: allows development of the
Phase 2 Development : provides for development single family detached units) in addition to Phase
Phase 3 Development : provides for development single family detached units) in addition to Phase
Required Improvements:
I-485 Southbound Ramp & Providence R
hase 2 Conditions
nese suggested improvements are needed under tersection 3:
Extend the existing westbound left turn lane fiExtend the existing westbound right turn lane
hase 3 Conditions
• No additional suggested improvements.
I-485 Northbound Ramp & Providence R
hase 2 Conditions
• No suggested improvements.

Phase 2 Conditions

and detached homes/lots being developed prior to issuance of the certificate • Westbound - A combined left-thru-right turn lane. nit developed on the Site. • Northbound - A combined left-thru-right turn lane. • Southbound - A combined left-thru-right turn lane. nd Multimodal/Pedestrian Improvements: Middle School Driveway We propose the following driveway configuration: nent. The Petitioner may provide or cause to be provided on its own or in ment portions of the improvements, the improvements set forth below in • Eastbound - Two ingress lanes and one egress lane (a channelized free-flow eastbound right turn lane). rovisions. It is understood the following section and specific improvements • Northbound - A combined northbound thru-left turn lane. sportation Impact Study (the "TIS"). • Southbound - A combined southbound thru-right turn lane. itially completed and approved by CDOT and/or NCDOT, as applicable, Substantial Completion. Reference to "substantial completion" for certain improvements as set forth in the provisions is issued in connection with the development associated with the applicable of Section 4.I above shall mean completion of the roadway improvements in accordance with the standards set forth in Section y and corresponding to the transportation improvements for *Phase 1, 2 or 3* ject to adjustments as may be allowed by CDOT and, if applicable NCDOT, 4.I.a. and b. above provided, however, in the event certain non-essential roadway improvements (as reasonably determined by CDOT) are not completed at the time that the Petitioner seeks to obtain a certificate of occupancy for building(s) on the Site in s Section 4): connection with the development improvements described above, then CDOT may instruct applicable authorities to allow the issuance of certificates of occupancy for the applicable buildings, and in such event the Petitioner may be asked to post a letter **o** ION PROVISIONS OF THIS SECTION 4.1 ARE SUBJECT TO of credit or a bond for any improvements not in place at the time such a certificate of occupancy is issued to secure completion AND CONSIDERATION AS PART OF THE ON-GOING of the applicable improvements. **PLETION AND REVIEW** d. Right-of-way Availability. It is understood that some of the public roadway improvements referenced in subsection middle school and associated uses contemplated for Development Area B 4.I.a. and b. above may not be possible without the acquisition of additional right of way. If after the exercise of **b**. The Petitioner shall dedicate 46 feet of right-of-way from the existing centerline of Tom Short Road and shall provide diligent good faith efforts over a minimum of a 60-day period, the Petitioner is unable to acquire any land necessary to for the cross-section improvements as generally depicted on the Rezoning Plan. t of up to 390 units (whether multi-family units, single family attached units provide for any such additional right of way upon commercially reasonable terms and at market prices, then CDOT, the City of Charlotte Engineering Division or other applicable agency, department or governmental body may agree to c. An eight (8) foot planting strip and twelve (12) foot multi-use path shall be provided along the Site's frontage on Tom se 1: and proceed with acquisition of any such land. In such event, the Petitioner shall reimburse the applicable agency, Short Road. department or governmental body for the cost of any such acquisition including compensation paid by the applicable at of up to 527 units (whether multi-family units, single family attached units agency, department or governmental body for any such land and the expenses of such proceedings. Furthermore, in **d.** An eight (8) foot planting strip and eight (8) foot sidewalk shall be provided along the Site's frontage on Golf Links se 1 and Phase 2 development the event public roadway improvements referenced in subsection a. above are delayed because of delays in the acquisition of additional right-of-way as contemplated herein and such delay extends beyond the time that the Petitioner seeks to obtain a certificate of occupancy for building(s) on the Site in connection with the development improvements described above, then CDOT will instruct applicable authorities to allow the issuance of certificates of e. oad (Signalized) complete the applicable road-way improvements; in such event the Petitioner may be asked to post a letter of credit or and/or other pedestrian features. The minimum width for these internal sidewalks shall be a minimum of six (6') feet. a bond for any improvements not in place at the time such a certificate of occupancy is issued to secure completion of the applicable improvements. the No Build conditions and are suggested in lieu of improvements at In addition to the foregoing, in the event the right of way is not available for any reason after the good faith efforts of the facilities may be installed within this buffer area. In addition, improvements to the buffer may be made as allowed by City Petitioner, the Petitioner will work with CDOT and/or NCDOT as applicable to either (i) identify alternative improvements to Storm Water services regulations and ordinances. Plantings may be adjusted to reflect these improvements. implement traffic mitigation in lieu of the improvements impacted by the lack of right of way as described in subsection f. from 150 to 400 feet of storage. below; or (ii) contribute to CDOT and/or NCDOT as applicable, an amount equal to the estimated cost of the road 7. e from 150 to 300 feet of storage. improvements not completed due to the lack of available right of way, such funds to be used to complete such alternative roadway improvements in the general area of the Site in a manner reasonable agreeable to the Petitioner, CDOT and/or a. The location, size, and type of storm water management systems depicted on the Rezoning Plan are subject to review *NCDOT as applicable.* e. <u>Right-of-way</u> <u>Conveyance</u>. The Petitioner will dedicate via fee simple conveyance any additional right-of-way natural site discharge points. indicated on the Rezoning Plan as right-of-way to be dedicated, the additional right-of-way will be dedicated prior to oad (Signalized) of the proposed sidewalks located along the public streets located outside of the right-of-way where ROW dedication is Stormwater Ordinance. not provided. The permanent sidewalk easement will be located a minimum of two (2) feet behind the sidewalk where feasible. f. Alternative Improvements. Changes to the above referenced roadway improvements can be approved through the Administrative Amendment process upon the determination and mutual agreement of Petitioner, CDOT and the Planning Director; provided, however, the proposed alternate transportation improvements provide (in the aggregate) comparable transportation network benefits to the improvements identified in this Petition in the overall area of the right turn movemen For example, transportation improvements may be substituted and/or payment in lieu for other improvements in the overall area of the rezoning may be implemented as described in subsection (d) above. Providence Road & Golf Links Drive (Signalized) g. <u>Roadway Standards.</u> All public roadway improvements will be subject to the standards and criteria of CDOT and 8. NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood roadway projects taking place within the broad southern Mecklenburg area, by way of a private/public partnership effort or other public sector project support. • Extend the existing westbound left turn lane from 150 to 400 feet of storage. II. Access, Streetscape and Setbacks. a. Access to the Site will be from Tom Short Road and an extension of Golf Links Drive as well as other public street trails, seating areas and other amenities), a club house or other amenity areas designed to be used and enjoyed by the residents extensions and connections made from the Site and into the Site from adjoining properties as generally depicted on the and guests of the community. Passive open space areas will be environmental areas such as tree save areas, water quality Rezoning Plan. b. The Petitioner will request a Subdivision Street Exception as allowed by Section 20-23(d) of the Subdivision 9. Signage regulations to convert the required public street extension of Camp Verde Ln. from a public street extension to a connection of Camp Verde Ln. into the Site via a multi-use path. c. The Petitioner will work with CDOT and the adjacent Stone Creek Ranch residents to explore the possibility of funding traffic calming devices on the existing public streets located along the Site's southern property line (Turning Hawk Rd., Mesa Verde Rd. and Raffia Rd.). **d.** The new internal network required public streets will be designed and built to the Residential Wide Street standards. e. The new internal network required public streets, except for Golf Links Dr. extension, will be improved with an eight (8) foot planting strip and a six (6) foot sidewalk. Golf Links Drive extension will be improved with an eight (8) foot planting Ardrey Kell Road & Sandy Rock Place (Signalized) strip and an eight (8) foot sidewalk. f. The Site's frontage along Tom Short Rd. will be improved with an eight (8) foot planting strip and a 12-foot multi-use g. A 16-foot setback as measured from the back of the proposed curb will be established along the internal network required public streets for all the proposed uses. h. Along Tom Short Rd. the following setbacks will be established: (i) for Development Area B a 50-foot setback as 12. Amendments to the Rezoning Plan: measured from the future back of curb will be provided; and (ii) for Development Area A a 30-foot setback as measured from the future back of curb will be provided. The number and location of access points to external properties/streets shall be provided as generally depicted on the Rezoning Plan and to the internal public streets will be determined during the building permit process and thereafter additional or fewer driveways and/or additional private/public streets may be installed or removed with approval from appropriate governmental authorities subject to applicable statutes, ordinances and regulations. The alignment of the internal vehicular circulation and driveways may be modified by the Petitioner to accommodate changes in traffic patterns, parking layouts and any adjustments required for approval by CDOT in accordance with published k. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements. III. Pedestrian Improvements. Sheet RZ-6 generally depicts the internal pedestrian/trail network associated with the master plan for the Site contemplated but adjustments to this internal pedestrian/trail network may be made as long as the overall design intent is substantially maintained. In addition, Sheet RZ-6 generally depicts broader external pedestrian network, some components of which currently exist, to provide a vision for possible overall pedestrian connectivity in the area, but it is understood that this broader network is aspirational in nature and is not to be considered to set forth commitments to specific pedestrian improvements. 5. General Architectural Standards and Parking Location Restrictions: **a.** Vinyl as a building material will not be allowed except on windows and soffits. **b.** The following standards shall apply to the multi-family residential units: Tom Short Road & Polo Ridge Elementary School Driveway-Bus (Unsignalized) i. Building Placement and Site Design shall focus on and enhance the pedestrian environment through the following: Buildings shall be placed so as to present a front or side façade to all network required streets (public or private) Buildings shall front a minimum of sixty (50%) percent of the total network required street frontage on the site (exclusive of driveways, pedestrian access, points, accessible open space, tree save or natural areas, tree replanting areas and storm water facilities) ii. Parking lots shall not be located between any building and any network required public or private street; and c. Building Massing and Height shall be designed to break up long monolithic building forms as follows: . Buildings exceeding one hundred twenty (120') feet in length shall include modulations of the building massing/facade plane (such as recesses, projections, and architectural details); and ii. Modulations shall be a minimum of ten (10') feet wide and shall project or recess a minimum of six (6) feet extending through the building. Attached Dwelling unit buildings fronting public or private network required streets shall be limited to five (5) individual units or fewer. The number of individual units per building shall be varied in adjacent buildings if multiple five unit buildings are adjacent to each other. **e.** The following standards shall apply to attached dwelling unit: i. To provide privacy, all residential entrances within fifteen feet of the sidewalk must be raised from the average sidewalk grade a minimum of twenty-four inches; ii. Pitched roofs, if provided, shall be symmetrically sloped no less than 4:12, except that roofs for porches and attached sheds may be no less than 2:12, unless a flat roof architectural style is employed; iii. Usable porches and stoops shall form a predominant feature of the building design and be located on the front and/or side of the building; of storage) on proposed Access "A" iv. Usable front porches shall be covered and be at least six (6) feet deep. Stoops and entry-level porches shall be covered and be at least three feet deep but shall not be enclosed; v. All corner/end units that face a public or private street shall have a porch or stoop that wraps a portion of the front appropriate pavement markings to allow for a two-way left turn lane at Coral Rose Road. and side of the unit or provide blank wall provisions that limit the maximum blank wall expanse to ten feet on all building levels; b. Additional Required Improvements: [NOTE: SUBJECT IN PARTICULAR TO FURTHER REVIEW] vi. Walkways shall be provided to connect all residential entrances to sidewalks along public and private streets;

Phase 3 Conditions • Channelize the northbound right turn lane to operate free from the signal under yield conditions. The following improvements are suggested at Intersection 1 in lieu of improvements at this study intersection: • Extend the existing westbound right turn lane from 150 to 300 feet of storage. • No suggested improvements. Phase 2 Condition • Install APS pushbuttons and upgrade any ramps that are out of PROWAG/ADA compliance. Phase 2 Conditions No suggested improvements. Phase 3 Conditions No suggested improvements. 6. Red Rust Lane & Golf Links Drive (Unsignalized) Phase 2 Conditions No suggested improvements. • Install a traffic signal. 7. Red Rust Lane & Ardrey Kell Road (Signalized) Phase 2 Conditions • No suggested improvements Phase 3 Conditions No suggested improvements. 8. <u>Tom Short Road & Ardrey Kell Road (Signalized)</u> • No suggested improvements. *Phase 3 Conditions* No suggested improvements. Phase 1 Condition • No suggested improvements. 0. <u>Tom Short Road & Polo Ridge Elementary School Driveway-Out (Unsignalized)</u> Phase 1 Conditions • Install a traffic signal. 11. Tom Short Road & Polo Ridge Elementary School Driveway-In (Unsignalized) No suggested improvements. • Construct a westbound right turn lane on Tom Short Road with 300 feet of storage. Phase 3 Conditions • No additional suggested improvements. acres). Therefore, the Petitioner requests an increase in the number of allowed units from 836 units to 917 units (an • One ingress lane and two egress lane (a terminating westbound right turn lane and a separate left turn lane with 275 feet • Install a traffic signal. • Construct a northbound right turn lane with 275 feet of storage. • Construct a southbound left turn lane with maximized storage to start at the Flat Branch Creek crossing and provide the

v. Single-family detached lots may front on public or private streets (if private streets are used, they will not be gated) or

Phase 1 Conditions

lane should serve as the turn lane into the bus lot).

Internal Intersection "A" (Residential Only)

The following configurations are recommended: • Eastbound - A combined left-thru-right turn lane.

The internal intersection was analyzed to create a complete model and the following configurations are recommended as a part of the required school queueing and stacking analysis (volume assumptions and analysis can be found in the appendix and

• Eastbound - A combined thru-right turn lane and a separate left turn lane with 650 feet of storage (the extended left turn • Westbound - A combined thru-right turn lane and a separate left turn lane with 100 feet of storage. • Southbound - A combined thru-left turn lane and a separate right turn lane that terminates at the proposed middle school

viii. Service Area Screening - service areas such as dumpsters, refuse areas, recycling and storage shall be screened from view with materials and design to be compatible with principal structures.; ix.Meter banks will be screened from adjoining properties and from the abutting public streets; and

vii. Roof top HVAC and related mechanical equipment will be screened from public view at grade from the

x. Garage doors proposed along public or private streets shall minimize the visual impact by providing one or more of the following: Architectural treatments such as translucent windows and projecting elements over the garage door opening A garage door with windows and light fixtures on either side or above the garage door.

f. The following standards shall apply to single family detached units:

nearest street:

i. Vehicular entrances to garages shall be located to the rear of the building and shall not face a Main Street frontage, Primary Street frontage, or common open space. A residential alley shall not be considered a frontage;

iii. The primary pedestrian entry to each dwelling unit shall face a frontage or a common open space if the open space is a minimum of 20' in depth;

iv.To provide privacy, all residential entrances within 15 feet of the sidewalk must be raised from the average sidewalk grade a minimum of 12 inches, subject to applicable deviations to address site constraints; v. Pitched roofs, if provided, shall be symmetrically sloped no less than 6:12, except that roofs for porches and attached sheds may be no less than 2:12, unless a flat rood architectural style is employed; vi. Usable porches and stoops shall form a predominate feature of the building design and be located on the front and/or side of the building. Usable front porches are covered and are at least 4 feet deep. Stoops and entry-level

porches may be covered but not be enclosed; vii. All corner/end units that face a public or private street should have a porch or stoop that wraps a portion of the front and side of the unit or blank wall provisions shall be implemented that limit the maximum blank wall expanse to 10 feet on all building levels and/or allows acceptable landscaping treatments along such areas; iii. Sidewalks should be provided to connect one residential entrances to sidewalks along public and private streets;

ix.Buildings may orient to open space in lieu of a public street. Streetscape, Landscaping and Buffer:

a. Setbacks and yards as required by the MX-2 (Innovative) zoning district and as allowed by the Innovative Provisions above will be provided.

Extension.

Along the Site's internal public streets, the Petitioner shall provide a sidewalk and a cross-walk network that links all of occupancy for the applicable buildings; provided, however, Petitioner continues to exercise good faith efforts to the principal buildings on the Site with one another by way of links to sidewalks along the abutting public or private streets

> f. A 200' wide Class C buffer area will be provided along the northerly boundary of the Site as generally depicted on the Rezoning Plan; sidewalks/trails/bike paths and similar improvements as well as in Development Area A school recreation

Environmental Features:

and approval as part of the full development plan submittal and are not implicitly approved from and engineering perspective with this rezoning. Adjustments may be necessary in order to accommodate actual storm water treatment requirements and

the issuance of the first certificate of occupancy. The Petitioner will provide a permanent sidewalk easement for any **b**. The Site shall comply with the requirements of the City of Charlotte Tree Ordinance and the City Post Construction

c. A minimum of 15% of the Site will be provided as Tree Save areas as required by the Tree Ordinance.

The Petitioner shall provide stormwater detention controls 10% above the requirements of the Post Construction Stormwater Ordinance.

. Development within any SWIM/PCSO Buffer shall be coordinated with and subject to approval by Charlotte-Mecklenburg Storm Water Services and mitigate if required by City Ordinance. Petitioner acknowledges intermittent/perennial stream delineation reports are subject to review and approval upon submission of development plans for permitting and are not approved with rezoning decisions.

Open Space & Trails/Amenities:

that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or **a.** The Petitioner will provide a series of passive and active open space areas throughout the Site with an emphasis on Development Area D which shall include approximately 12-acres of natural preserve areas to include trails and other natural area related amenities (the exact location and configuration of these open space areas may vary; the final locations and configuration of the open space areas will be determined/finalized during each phase of the subdivision approval process). A minimum of [25] % of the Site will be provided as passive open space areas and a minimum of [20] % of the Site area will be provided and improved as active open space areas. Active open space areas will be areas improved with seating areas, trails, recreation fields, tennis courts, playgrounds, swimming pools, amenitized ponds (i.e. water quality ponds/areas improved with buffers, slopes, tree save areas, water quality areas or other open space areas of the community. It is understood passive open

> spaces within the tree save area shall adhere to the tree save standards. a. Signage as allowed by the Ordinance may be provided.

10. Lighting:

a. All new lighting shall be decorative, capped, and downwardly directed.

b. Detached lighting on the Site, except streetlights located along public streets, will be limited to twenty-five (25') feet in

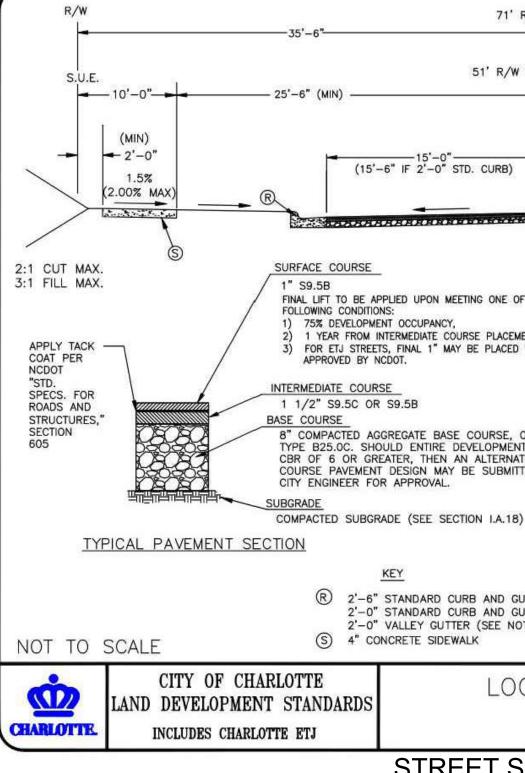
Greenway Dedication Flat Branch:

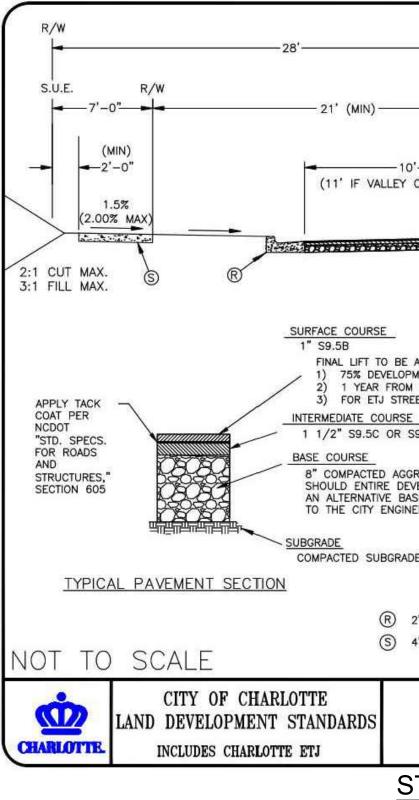
a. The Petitioner will convey to Mecklenburg County a 100-foot permanent greenway easement within the 200-foot post construction buffer for Flat Branch. The location of the 100-foot easement to be determined during the land development approval process for the Site. The 100-foot easement to be conveyed as development within each Development Area abutting Flat Branch occurs. Prior to the issuance of the first certificate of occupancy for each Development Area abutting Flat Branch the corresponding 100-foot easement will be conveyed. The 100-foot easement within Development Area F will be conveyed when the easement within Development Area E is required to be conveyed.

a. Future amendments to the Rezoning Plan (which includes these Development Standards) may be applied for by the then Owner or Owners of the applicable Development Area or portion of the Site affected by such amendment in accordance with the provisions herein and of Chapter 6 of the Ordinance.

Binding Effect of the Rezoning Application:

a. If this Rezoning Petition is approved, all conditions applicable to the development of the Site imposed under the Rezoning Plan will, unless amended in the manner provided herein and under the Ordinance, be binding upon and inure to the benefit of the Petitioner and subsequent owners of the Site and their respective heirs, devisees, personal representatives, successors in interest or assigns.





		/	
		PLANTING STRIP	S 2:1 CUT MAX.
UPON MEETING ONE OF THE	NOTES: 1. VALLEY GUTTER IS	SEE NOTE 9	3:1 FILL MAX.
CCUPANCY, EDIATE COURSE PLACEMENT,	CITY ENGINEERING	IIN. WHEN LESS THAN 12 DV	ELLING UNITS PER ACRE
VAL 1" MAY BE PLACED WHEN		LK IS 8' MIN. WHEN 12 D.U. PAVEMENT SECTION DESIGN M	
5B		ED ON SPECIFIC TRAFFIC PAI ARDSCAPE) ALLOWED IN LIEU	
ATE BASE COURSE, OR 4" BCBC ENTIRE DEVELOPMENT HAVE A	PRIOR APPROVAL	FROM ENGINEERING, CDOT, A NDARD DETAILS #50.09A, B,	ND PLANNING.
R, THEN AN ALTERNATIVE BASE SIGN MAY BE SUBMITTED TO THE PROVAL.	REGARDING ON-ST HEAD-IN PARKING	TREET "HEAD-IN" PARKING, E IS PERMITTED BY NCDOT. FREQUENT DRIVEWAYS THAT	XCEPT IN ETJ WHERE NO
(SEE SECTION I.A.18)		전기에 온 것은 바람이 같이 많아? 것을 같아. 이 것이가 앉아지 않아 가지 않는 것 같아. 이 것이 같아.	J. NO BUMPOUTS PERMITTED IN
		OF RIGHT-OF-WAY WIDTHS NANCE SECTION 20-22(d).	REFER TO CITY OF CHARLOTTE
NDARD CURB AND GUTTER,	8. ZONING SETBACK	MEASURED FROM TOTAL R/W	-
NDARD CURB AND GUTTER, OR NDARD CURB AND GUTTER, OR LEY GUTTER (SEE NOTE 1) TE SIDEWALK	FOOT (MIN.) UP T NATURAL GRADES	DJACENT TO SIDEWALK SHALL O 1¼" PER FOOT (MAX.), E MAKE THIS REQUIREMENT IM IR MAY AUTHORIZE A SUITAB	KCEPT WHERE EXCESSIVE PRACTICAL. IN SUCH CASES,
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TREET SECTIO	<u> IN A-A'</u>		
N.T.S.			
€ 56' TOTAL R∕W			R/W
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	21 (
'-6" 		(MIN) 8'-0"-	(MIN) (MIN) →→→5'−0"→→ (→−1'−0"
	ALLEY CURB, OR IF E	(TJ)	1.5%
S	LOPE 3/8" PER FT.		(2.00% MAX)
		® /	S 2:1 CUT MAX.
		PLANTING STRIP	3:1 FILL MAX.
		SEE NOTE S	
APPLIED UPON MEETING ONE OF TH MENT OCCUPANCY,			
I INTERMEDIATE COURSE PLACEMENT, EETS, FINAL 1" MAY BE PLACED WHE		1. FOR EXPLANATION	DN OF RIGHT-OF-WAY
59.5B		SUBDIVISION OF	TO CITY OF CHARLOTTE DINANCE SECTION 20-22(d).
REGATE BASE COURSE, OR 4" BCI VELOPMENT HAVE A CBR OF 6 OR	3C TYPE B25.0C. GREATER. THEN	2. ZONING SETBAC R/W	KS MEASURED FROM TOTAL
SE COURSE PAVEMENT DESIGN MA' EER FOR APPROVAL.		SHALL BE GRAD	ADJACENT TO SIDEWALK DED TO ¼" PER FOOT (MIN.) R FOOT (MAX.), EXCEPT
DE (SEE SECTION 1.A.18)		WHERE EXCESS THIS REQUIREM	VE NATURAL GRADES MAKE ENT IMPRACTICAL. IN SUCH Y ENGINEER MAY AUTHORIZE
KEY		A SUITABLE GR	
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4" CONCRETE SIDEWALK			
			T
LOCAL RESIDI			STD. NO. REV.
TYP	PICAL SEC	HON	U-02 19
TREET SECTIO)N B-B'		
N.T.S.			

71' R/W (TOTAL)

51' R/W MINIMUM (52' IN ETJ)

(15'-6" IF 2'-0" STD. CURB)

SLOPE 3/8" PER FT.

	CHILDRESS KLIEN	١
	RZ #2023-046	
	CHARLOTTE, NC	
ANDDES	SIGN PROJ.# 1022250	
F	REVISION / ISSUA	NCE
NO.	DESCRIPTION	DATE
0	INITIAL REZONING SUBMITTAL	01/24/22
1	REZONING SUBMITTAL	09/11/23
2	REZONING SUBMITTAL	11/14/23
3	REZONING- S UBMITTAL	12/12/23
DE	SIGNED BY: KST AWN BY: JYK	
DR	ECKED BY: KST	

RZ-3

ORIGINAL SHEET SIZE: 36" X 48

NOT FOR CONSTRUCTION

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R/W

(NOTE 2)

1.5%

(2.00% MAX)

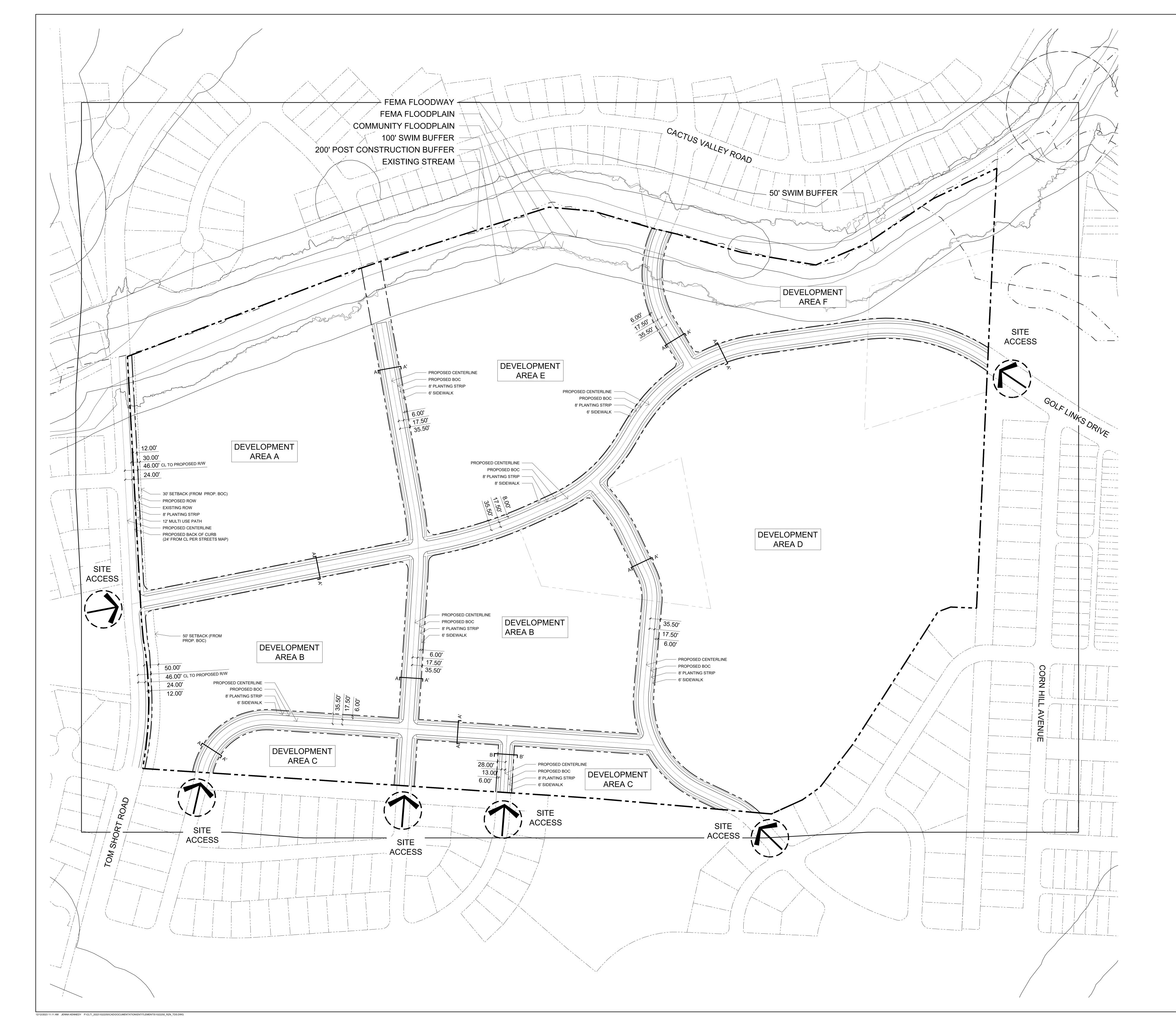
A. T. S. Conta

8'-0" VARIES | -1'-0"

(MIN)

223 NORTH GRAHAM STREE CHARLOTTE, NC 28202 704.333.0325

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 DATE

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CATO TRAILS

CHILDRESS KLIEN RZ #2023-046 CHARLOTTE, NC

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F	REVISION / ISSUANCE			
NO.	DESCRIPTION	DATE		
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1	REZONING SUBMIT	TAL 09/11/23		
2	REZONING SUBMIT	TAL 11/14/23		
3	REZONING-SUBMIT	TAL 12/12/23		
	DESIGNED BY: KST			
DRAWN BY: JYK CHECKED BY: KST				
SCALE NORTH				
COALL				
VERT: N/A HORZ: ########				
#	# ##	##		
SHEET TITLE				
PROPOSED STREET DIMENSIONS				

RZ-4



CONCEPTUAL CONNECTIVITY

- MAIN ACCESS POINTS
- ----- INTERNAL/EXISTING TRAIL CONNECTION
- POTENTIAL MULTI USE PATH
- PROPOSED SITE STREET NETWORK
- EXISTING STREET NETWORK
- **RESIDENTIAL NEIGHBORHOOD**
- COMMERCIAL USE

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PUBLIC OPEN SPACE

NOTE: THIS SHEET RZ-5 GENERALLY DEPICTS THE INTERNAL PEDESTRIAN/TRAIL NETWORK ASSOCIATED WITH THE MASTER PLAN FOR THE SITE CONTEMPLATED BUT ADJUSTMENTS TO THIS INTERNAL PEDESTRIAN/TRAIL NETWORK MAY BE MADE AS LONG AS THE OVERALL DESIGN INTENT IS SUBSTANTIALLY MAINTAINED. IN ADDITION, THIS SHEET RZ-6 GENERALLY DEPICTS BROADER EXTERNAL PEDESTRIAN NETWORK, SOME COMPONENTS OF WHICH CURRENTLY EXIST, TO PROVIDE A VISION FOR POSSIBLE OVERALL PEDESTRIAN CONNECTIVITY IN THE AREA, BUT IT IS UNDERSTOOD THAT THIS BROADER NETWORK IS ASPIRATIONAL IN NATURE AND IS NOT TO BE CONSIDERED TO SET FORTH COMMITMENTS TO SPECIFIC PEDESTRIAN IMPROVEMENTS.

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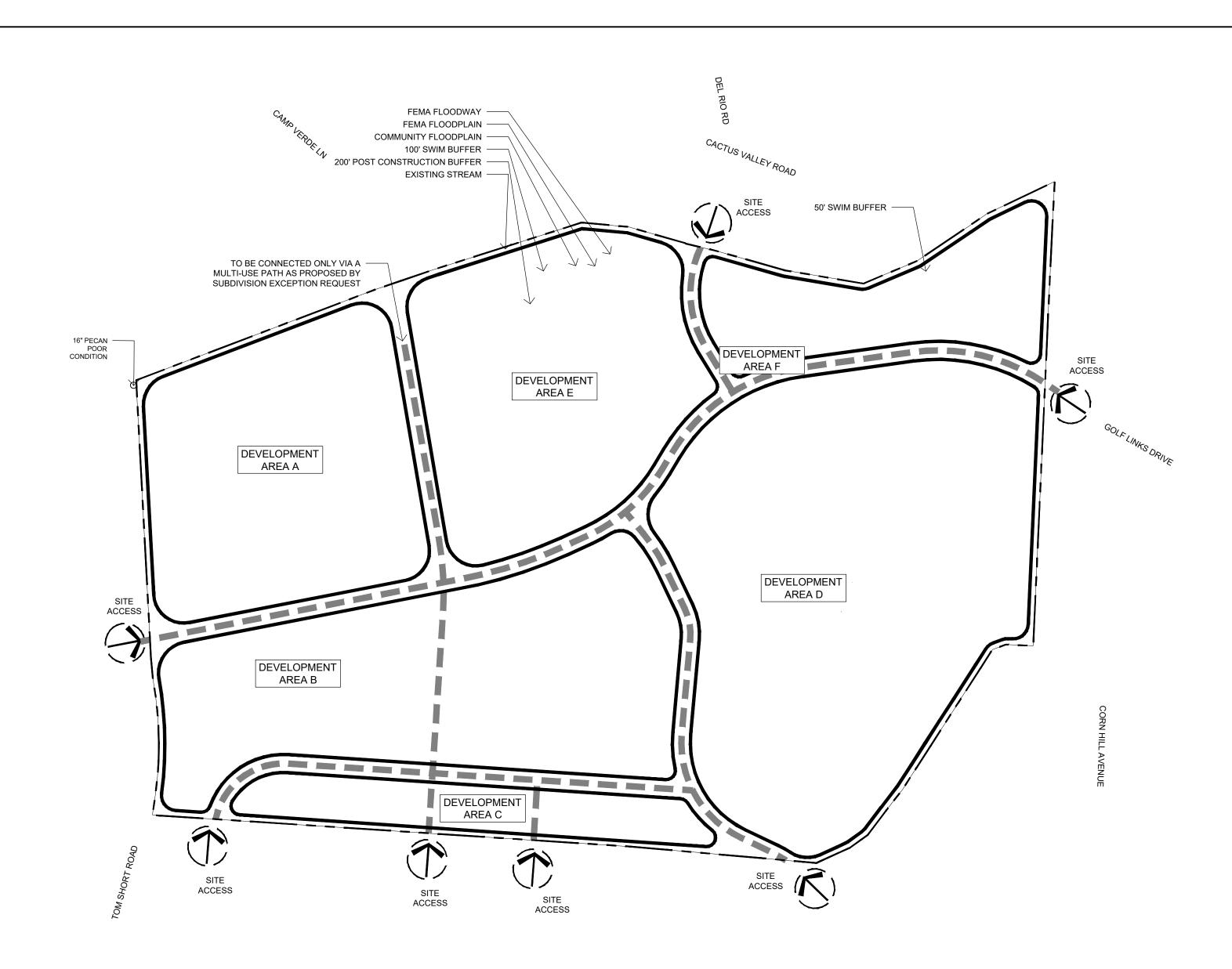
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CHILDRESS KLIEN RZ #2023-046 CHARLOTTE, NC

1022250 **REVISION / ISSUANCE** NO. DESCRIPTION INITIAL REZONING SUBMITTAL 01/24/22 REZONING SUBMITTAL 09/11/23 2 REZONING SUBMITTAL 11/14/23 ____ 3 REZONING-SUBMITTAL 12/12/23 DESIGNED BY: KST DRAWN BY: JYK CHECKED BY: KST $\mathbf{\hat{P}}$ VERT: N/A HORZ: CONCEPTUAL -ASPIRATIONAL ACTIVITY

RZ-5

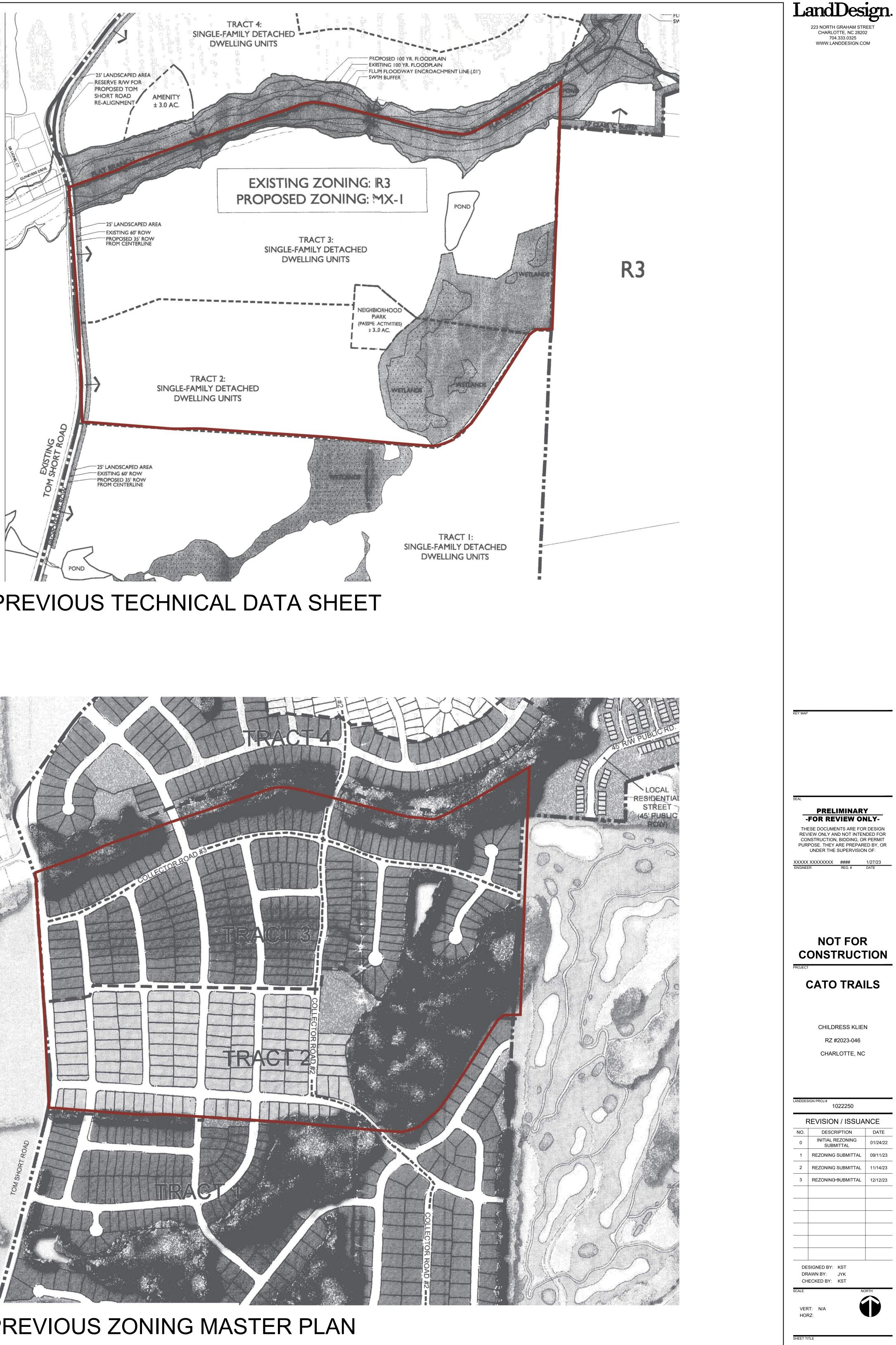


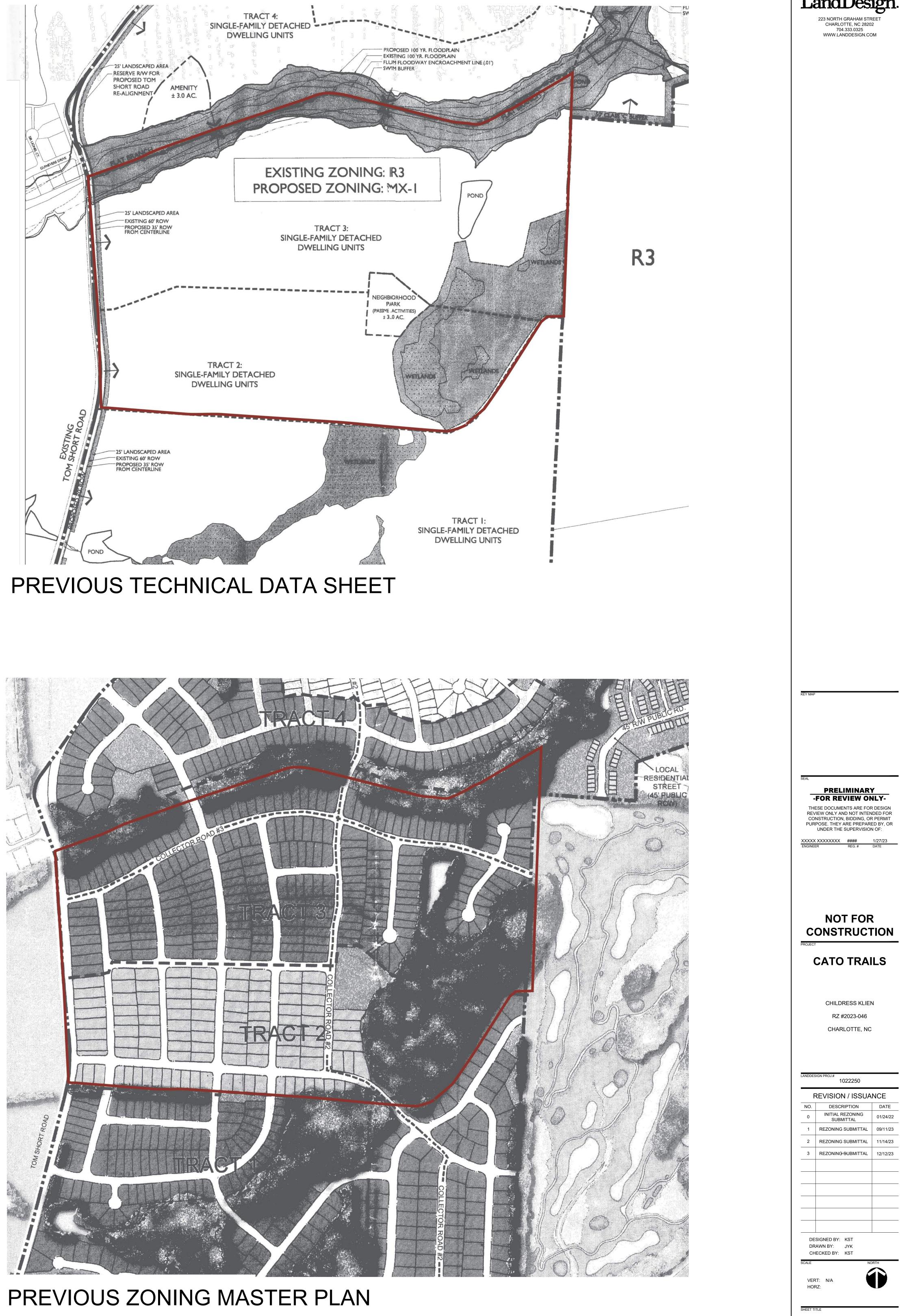
PROPOSED TECHNICAL DATA SHEET



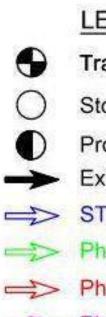
PROPOSED MASTER PLAN

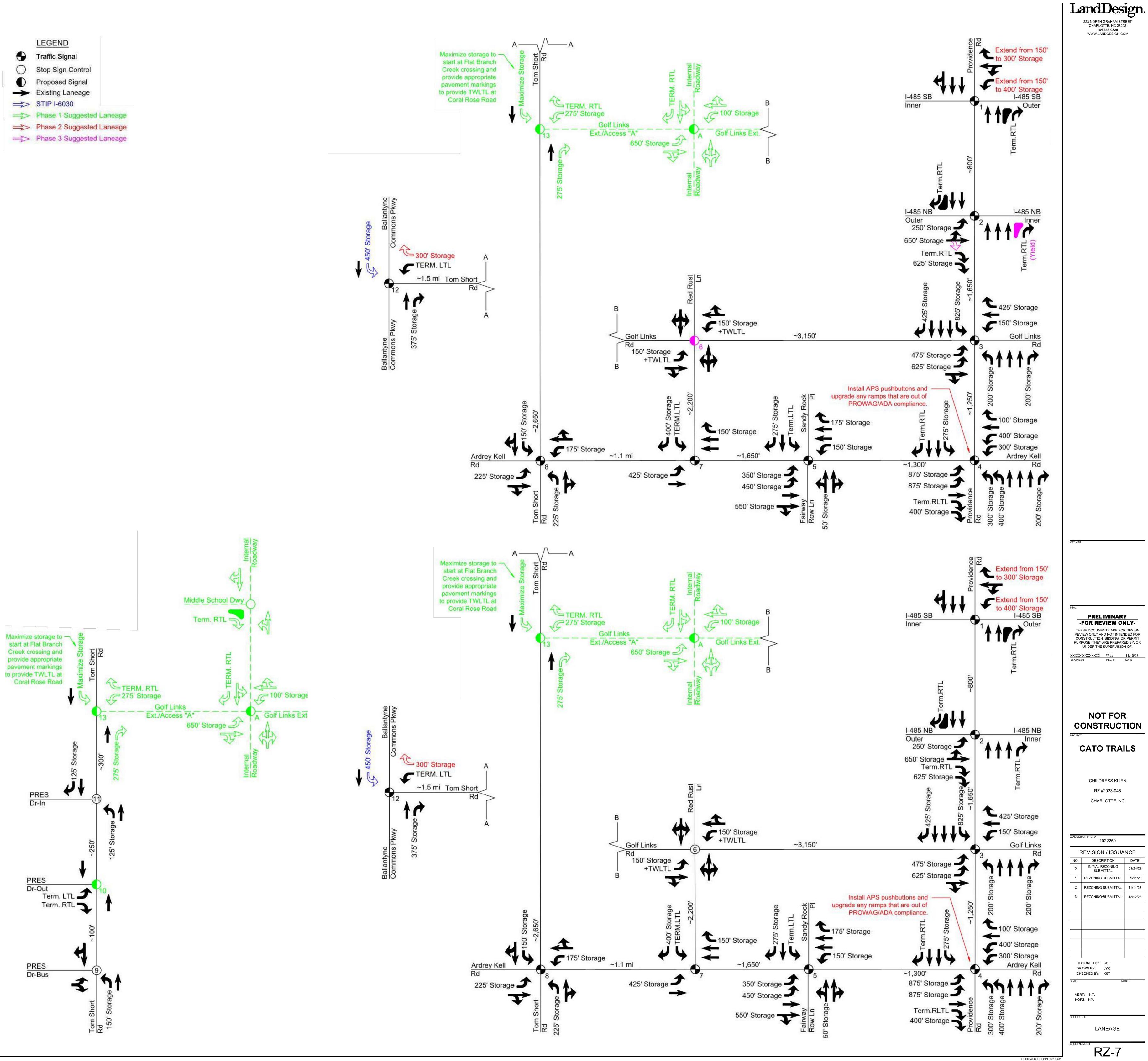
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RZ-6







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 DATE
 CONSTRUCTION **CATO TRAILS REVISION / ISSUANCE** DATE 01/24/22 REZONING SUBMITTAL 09/11/23