

Rezoning Transportation Analysis

Petition Number: 2023-155

General Location Identifier: 03714104, 03714101, 03714102, 03714103

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Revision Log:

Date	Description
12-27-23	First Review (IW)

General Review Information

The petition is located at the intersection of Trinity Road, a City-maintained major collector and Beatties Ford Road, a State-maintained major arterial. The petition is located in a northwest wedge.

Active Projects Near the Site:

- There are no known transportation projects in the vicinity

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The petition is located at the intersection of Trinity Road, a City-maintained major collector and Beatties Ford Road, a State-maintained major arterial. Based on the 1,065 daily trips, the petitioner will be required to satisfy Tier 1 Multimodal assessment (3 points). Site plan and/or conditional note revisions are needed to commit to installing curb and gutter in future location, completing Tier 1 of CTR, dedicating right of way per Article 32 in UDO, installing left and right turn lane(s), and adding conditional note(s). Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Single Family Warehouse	3 Dwelling Units 1,080 SF	65	<i>Tax Record</i>
Entitlement with Current Zoning	Retail Single Family (CG & N1-A, 16.154 acres)	65,300 SF 28 Dwelling Units	3,300	<i>General Guidance from Planning</i>
Proposed Zoning	Single Family Attached (N2-B, 16.154 acres)	146 Dwelling Units	1,065	<i>Site Plan: 11-17-23</i>

Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

Outstanding Issues

~~Strikethrough~~ = Resolved

1. Curblines:

- a. **Beatties Ford Road:** Location of future back of curb and gutter to be constructed 30' from centerline.

Cross-section from Streets Maps 4+ Avenue, Shared Use Path

- b. **Trinity Road:** Location of future back of curb and gutter to be constructed 17.5' from centerline.

CLDSM U-07B Local Collector Street

Label and dimension the curb and gutter from the centerline for each road on the site plan.

2. Comprehensive Transportation Review (CTR):

A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating site trips under ordinance thresholds found in the Charlotte Streets manual Table 3.1 and/or not triggering any other City TIS requirements.

The petition's zoning falls within the medium to high intensity development which based on the 1,065 daily trips will trigger Tier 1 (3 mitigation points) for multimodal assessment. The petitioner shall review and assess the publicly accessible pedestrian network within ¼ mile walking distance of the site to identify multimodal infrastructure to meet the Tier 1 (3 mitigation points) for multimodal assessment.

- 3. Revise site plan and conditional note(s) to commit to dedicate 33.5 feet of right-of-way from the Trinity Road centerline. The site plan shall label and dimension the right-of-way from the road centerline.
- 4. Revise site plan and conditional note(s) to commit to dedicate 50 feet of right-of-way from the Beatties Ford Road centerline. The site plan shall label and dimension the right-of-way from the road centerline.
- 5. Add a conditional note specifying "A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual,

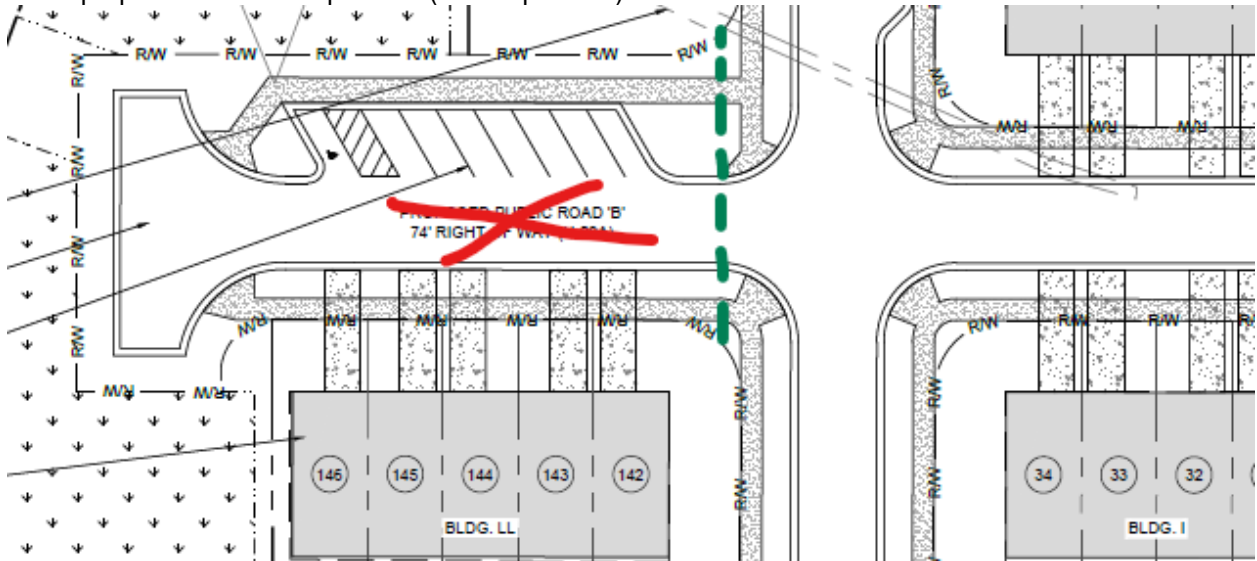
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group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements."

6. Per coordination with NCDOT, revise site plan and conditional note(s) to commit to constructing a left turn lane with 150' storage on Beatties Ford Road.
7. Per coordination with NCDOT, revise site plan and conditional note(s) to commit to constructing a right turn lane with 100' storage on Beatties Ford Road.
8. Revise site plan and conditional note(s) to commit to designating the residential hammerhead portion of the proposed road to be private. (see snip below)



9. CDOT REQUEST: Revise site plan and conditional note(s) to commit to installing a Left turn lane at the intersection from Beatties Ford onto Trinity Road.
10. CDOT REQUEST: Revise site plan and conditional note(s) to commit to reconfiguring the site so that homes are alley fed which will decrease conflict points on the public road. At minimum, revise site plan to alley feed all units along Public Road 'A' and 'E'.
11. Revise site plan and conditional note(s) to commit to removing conditional note 3.D. as driveways will be reviewed during the permitting process as site specific alternations could change or dictate the specific dimensions needed as shown in the Charlotte Land Development Standards Manual. If petitioner decides to make units accessible by alley, CDOT is comfortable with this note residing.

D. PROPOSED INDIVIDUAL DRIVEWAYS TO BE PERMITTED AS SHOWN ON REZONING PLAN. DRIVEWAY WIDTHS TO BE 10' AND MAINTAIN 20' BEHIND PROPOSED SIDEWALKS.

12. Add a conditional note specifying "the Petitioner shall dedicate and convey in fee simple all rights-of-way to the City before the site's first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible."
13. Add a conditional note specifying "All transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued."

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The petitioner may phase transportation improvements if said improvements and phasing are approved by CDOT and explicitly described in site plan notes.

14. Add conditional note specifying “All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or roadway projects taking place within the broad north western Mecklenburg area, by way of a private/public partnership effort or other public sector project support.”

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte Unified Development Ordinance.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and to comply with City of Charlotte Unified Development Ordinance regulations.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>