

# Rezoning Transportation Analysis

Petition Number: 2023-094

General Location Identifier: 02930129, 02930130

**From: Jake Carpenter, PE**  
Jacob.Carpenter@charlottenc.gov  
980-221-5675

**Reviewer: Travis Miller**  
Travis.Miller@charlottenc.gov  
980-221-5778

## Revision Log:

Date	Description
9-28-23	First Review (TM)
10-26-23	Second Review (TM)
11-30-23	Third Review (ME)

## General Review Information

The petition is located adjacent to Johnston Oehler Road, a City-maintained minor arterial, east of Rumble Street, a privately-maintained local street. The petition is located in a northeast wedge outside of Route 4, within the Prosperity Hucks Area Plan.

## Active Projects Near the Site:

- N/A

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*CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.*

*This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.*

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Based on our review of the petition, we offer the following information for your consideration.

## Transportation Summary

The site is located adjacent to Johnston Oehler Road, a City-maintained minor arterial, east of Rumble Street, a privately-maintained local street. A Traffic Impact Study (TIS) is not required for the site due to the development generating site trips under ordinance thresholds and not meeting additional TIS triggers. All outstanding issues have been resolved.

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## Trip Generation

Scenario	Land Use	Intensity	Trip Generation			Source
			Daily Trips	AM Peak Hour Trips	Pm Peak Hour Trips	
Existing Use	Single Family Detached	2 Dwelling Units	20	2	3	Tax Record
Entitlement with Current Zoning	Single Family (N1-A, 3.52 acres)	10 Dwelling Units	125	9	11	General Guidance from Planning
Proposed Zoning	Single Family Attached (N1-F, 3.52 acres)	45 Dwelling Units	295	18	23	Site Plan: 06-15-23
Proposed Zoning	Single Family Attached (N1-F, 3.52 acres)	46 Dwelling Units	300	18	24	Site Plan: 06-15-23
Proposed Zoning	Single Family Attached (N1-F, 3.52 acres)	45 Dwelling Units	295	18	23	Site Plan: 11-15-23

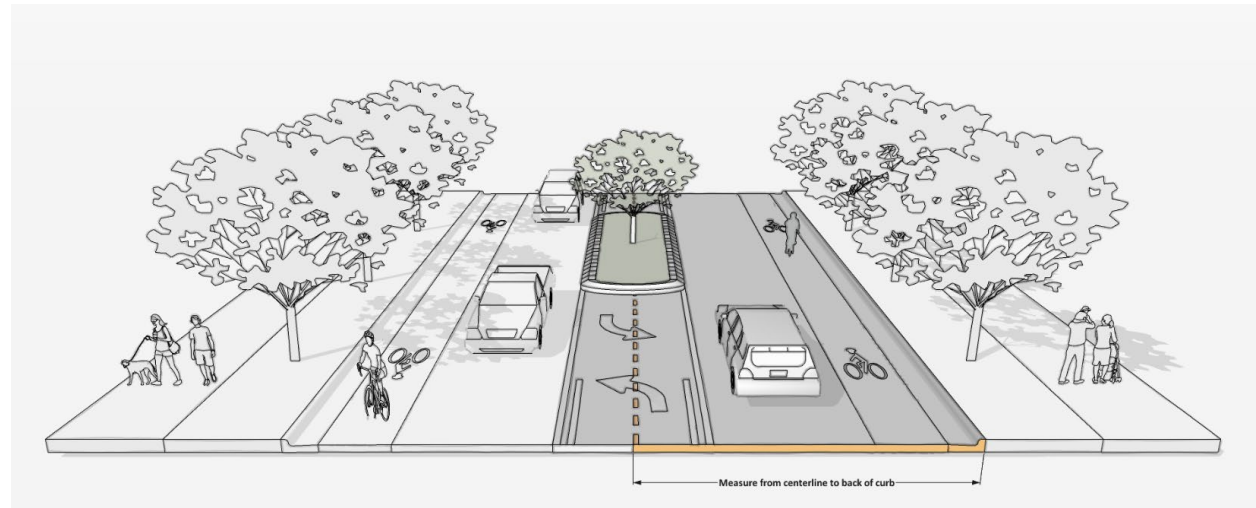
**Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.**

### Outstanding Issues

**Strikethrough = Resolved**

**1. ~~Curbline:~~**

- a. ~~Road Name (Johnston Oehler Road): the future location of curb and gutter is in its existing location.~~



~~Label and dimension the curb and gutter from the centerline for each road on the site plan.~~

**2. ~~Comprehensive Transportation Review (CTR):~~**

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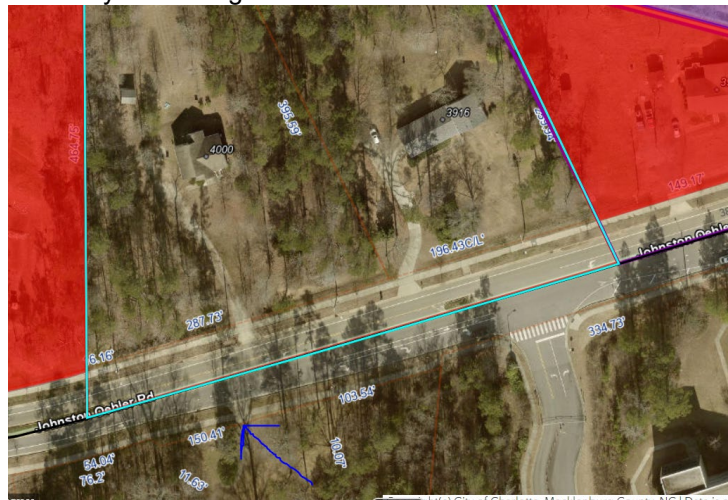
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A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating site trips under ordinance thresholds found in the Charlotte Streets manual Table 3.1 and/or not triggering any other City TIS requirements.

3. ~~Revise site plan and conditional note(s) to commit to dedicating a min of 35 feet of right of way from the road centerline. The site plan shall label and dimension the right of way from the road centerline.~~
4. ~~Add a conditional note specifying "A Right of Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right of way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements."~~
5. **Outstanding Comment from 9-28-23:** Revise site plan and conditional note(s) to commit to lining up driveway with driveway on the Southern side of the roadway.

**Clarifying Comment from 10-26-23:** Site access is to line up with the residential driveway on the southern side of the roadway. See image below.



6. **Outstanding CDOT REQUEST from 09-28-23:** Revise site plan and conditional note(s) to commit to installing a signalized pedestrian crossing at the existing pedestrian crossing location that would allow for connection to Mallard Creek High School.
7. ~~Add a conditional note specifying "the Petitioner shall dedicate and convey in fee simple all rights-of-way to the City before the site's first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible."~~
8. ~~Add a conditional note specifying "All transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued."~~

The petitioner may phase transportation improvements if said improvements and phasing are approved by CDOT and explicitly described in site plan notes.

9. **New Comment 10-26-23:** Remove conditional note referencing reverse angle parking

5. THE PETITIONER AGREES TO COORDINATE WITH CDOT ON ALL APPROPRIATE SIGNAGE RELATED TO THE PROPOSED REVERSE ANGLE PARKING DURING THE PERMITTING PROCESS.

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## Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte Unified Development Ordinance.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and to comply with City of Charlotte Unified Development Ordinance regulations.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>