

Rezoning Transportation Analysis

Petition Number: 2023-080

General Location Identifier: 16719249

From: Jake Carpenter, PE
Jacob.carpenter@charlottenc.gov
980-221-5675

Reviewer: Patrick Monroe
Patrick.Monroe@charlottenc.gov
704-301-1411

Revision Log:

Date	Description
8-24-23	First Review (JP)
9-20-23	Second Review (JP)
10-26-23	Third Review (JP)
12-20-23	Fourth Review (JP)
1-24-24	Fifth Review (JP)

General Review Information

The petition is located at the intersection of Forest Point Boulevard, a City-maintained major collector, and Nations Ford Road, a City-maintained minor arterial. The petition is located in a south corridor outside of Route 4, within the Westside Strategy Plan Study Area.

Active Projects Near the Site:

- I-77 South Widening (U-5718)
 - Direct-connect access for Arrowood is being studied which could have potential impacts to this parcel.
- Capital Improvement Project – Storm Drainage Improvement Project at 7500 Forest Point Blvd
 - Install and/or repair drainage infrastructure.

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located at the intersection of Forest Point Boulevard, a City-maintained major collector, and Nations Ford Road, a City-maintained minor arterial. A Traffic Impact Study (TIS) is not required for this site due to the site not triggering CTR thresholds. All outstanding CDOT comments have been addressed.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation			Source
			Daily Trips	AM Peak Hour Trips	PM Peak Hour Trips	
Existing Use	Vacant	-	0	0	0	Tax Record
Entitlement with Current Zoning	Single Family Detached (R-9, 19.16 acres)	37 Dwelling Units	405	30	39	General Guidance from Planning
Proposed Zoning	Single Family Attached (N2-A, 19.16 acres)	78 Dwelling Units	545	35	43	Site Plan: 3/31/2023
Proposed Zoning	Single Family Attached (N2-A, 19.16 acres)	76 Dwelling Units	530	34	42	Site Plan: 3/31/2023
Proposed Zoning	Single Family Attached (N2-A, 19.16 acres)	72 Dwelling Units	500	32	39	Site Plan: 11/22/2023

Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

Outstanding Issues

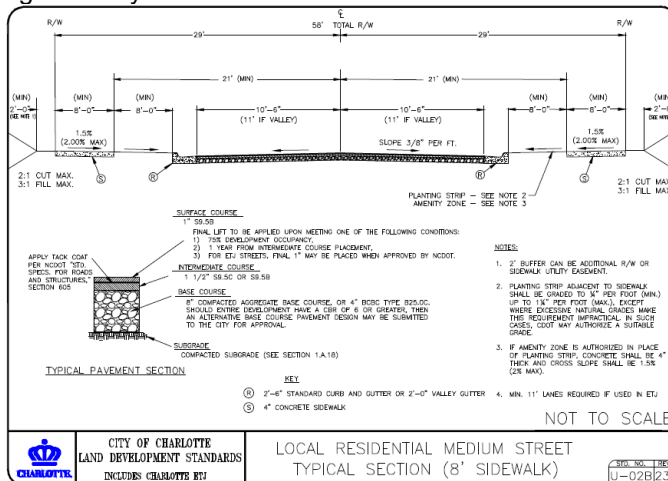
Strikethrough = Resolved

1. ~~Curbline:~~

a. ~~Forest Point Boulevard:~~ Future curb line is in the existing location

Clarifying Comment 10.26.23: From the centerline of Forest Point, label and dimension the ROW and back of curb.

b. ~~Fawn Drive Extension:~~ Street typical section is local residential medium (U-02B) with 58' of right-of-way.

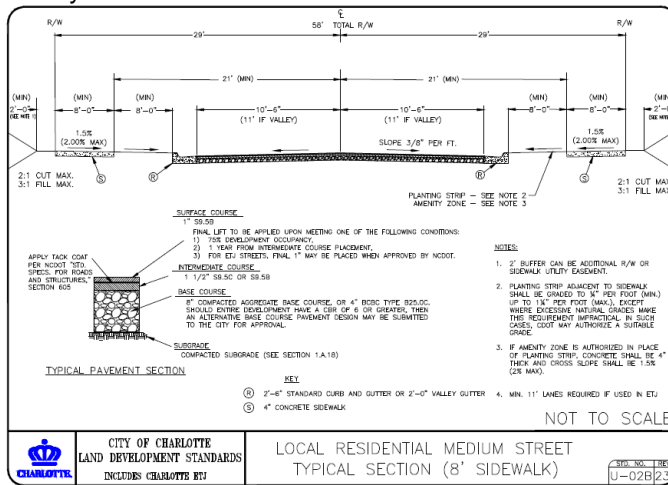


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c. ~~Proposed Public Street: Cross section is local residential medium (U-02B) with 58' of right-of-way.~~



Clarifying Comment 9.20.23: U-02B should be used based on the zoning district N2 and street type.

NOTE: Additional distance from center line to back of curb may be required in locations where a median is present or around intersections and interchanges.

Label and dimension the curb and gutter from the centerline for each road on the site plan.

2. ~~Comprehensive Transportation Review (CTR):~~

A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating site trips under ordinance thresholds found in the Charlotte Streets manual Table 3.1 and/or not triggering any other City TIS requirements.

3. ~~Revise site plan and conditional note(s) to commit to construct an 8-foot planting strip, and 6-foot sidewalk on Forest Point Boulevard per UDO 32.7(D)(b)(ii); 8-foot planting strip, and 8-foot sidewalk on both sides of Fawn Drive Extension per UDO 33.3(D); and); 8-foot planting strip, and 8-foot sidewalk on both sides of Proposed Public Street per UDO 33.3(D). The site plan shall label and dimension both items from the back of curb and gutter or edge of pavement.~~

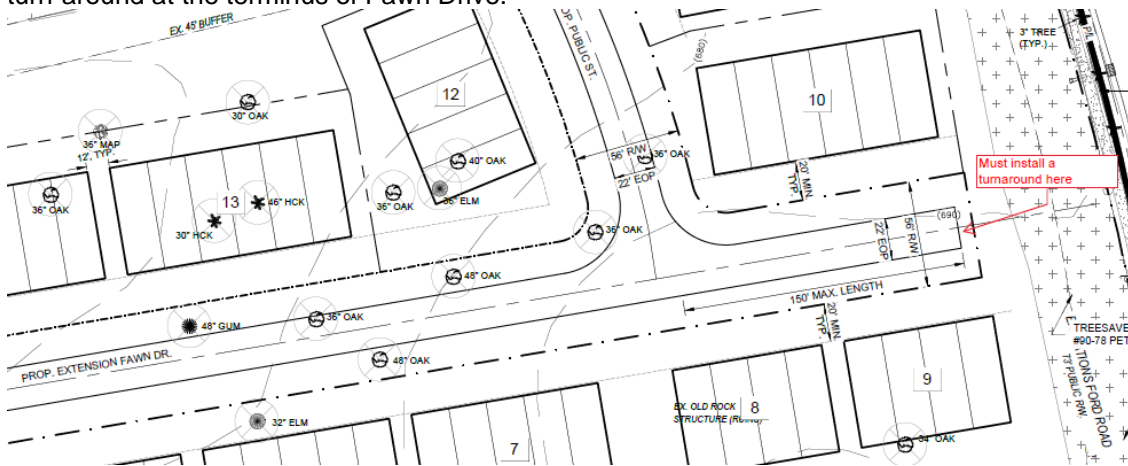
Clarifying Comment 9.20.23: 1) It is ordinance requirement, including 6' sidewalks in N1, 2) responsibility for adjustments from the ordinance requirement for sidewalk fall under the planning department and would occur during permitting 2) sidewalk cost reimbursement is available for affordable housing developments that meet the City's affordable housing criteria.

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4. ~~Revise site plan and conditional note(s) to commit to extending the bike lane on Forest Point Drive to the traffic signal at Nations Ford Road. Modify the existing curb line as needed to incorporate the bike lane.~~
5. ~~Add a conditional note specifying "A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements."~~
6. ~~CDOT REQUEST: CDOT would prefer to see that the driveways were alley fed from the public street rather than direct access.~~
7. **Outstanding Comment 9.20.23:** Revise site plan and conditional note(s) to commit to installing a turn-around at the terminus of Fawn Drive.



8. ~~Add a conditional note specifying "All transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes."~~

Clarifying Comment 9.20.23: This should be before the "First" CO, not the "Thirty-First".

- 5.i. ALL TRANSPORTATION IMPROVEMENTS WILL BE APPROVED AND CONSTRUCTED BEFORE THE ~~THIRTY-FIRST (31)~~ BUILDING CERTIFICATE OF OCCUPANCY IS ISSUED. THE PETITIONER MAY PHASE TRANSPORTATION IMPROVEMENTS IF SAID IMPROVEMENTS AND PHASING ARE EXPLICITLY DESCRIBED IN SITE PLAN NOTES.

9. **New Comment 10.26.23:** 35'x35' sight triangles are no longer in the sight distance policy, this should be 50'x50' sight triangles.

- 5.f. ~~SHALL MEET, PER THE CITY OF CHICAGO'S CURRENT REQUIREMENTS.~~
ADEQUATE SIGHT TRIANGLES MUST BE RESERVED AT THE EXISTING/PROPOSED STREET ENTRANCE(S). TWO 35' X 35' SIGHT TRIANGLES (AND TWO 10' X 70' SIGHT TRIANGLES ON NCDOT-MAINTAINED STREETS) ARE REQUIRED FOR THE ENTRANCE(S) TO MEET REQUIREMENTS. ALL PROPOSED TREES, BERMS, WALLS, FENCES, AND/OR IDENTIFICATION SIGNS SHALL NOT INTERFERE WITH SIGHT DISTANCE AT THE ENTRANCE(S) AND SHALL BE IDENTIFIED ON THE SITE PLAN.

Advisory Information

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The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>