

Rezoning Transportation Analysis

Petition Number: 2023-042

General Location Identifier: 11907801

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Reviewer: Patrick Monroe
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Revision Log:

Date	Description
07-26-23	First Review (PM)
08-23-23	Second Review (PDM)
11-27-23	Third Review (JP)
12-20-23	Fourth Review (JP)
03-20-24	Fifth Review (JP)
04-02-24	Trip Generation Update v1
04-15-24	Trip Generation Update v2
04-23-24	Sixth Review (JP)

General Review Information

The petition is located at the intersection of West Boulevard, a State-maintained major arterial and Mint Street, a City-maintained minor arterial. The petition is located in a south corridor inside of Route 4, within the Center City 2020 Vision Plan.

Active Projects Near the Site:

- N/A

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located at the intersection of West Boulevard, a State-maintained major arterial and Mint Street, a City-maintained minor arterial. A Traffic Impact Study (TIS) is not required for this site due to the site generating less than 2,500 daily trips and not triggering any other City TIS requirements. CDOT has coordinated with the petitioner to install APS pushbuttons at West Boulevard and South Mint Street. All outstanding CDOT comments have been addressed.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	-	-	Tax Record
Entitlement with Current Zoning	Townhomes N-1D (HDO), 3.28 acres	26 Dwelling Units	190	General Guidance from Planning
Proposed Zoning	Multifamily (Mid-Rise) Fast Food w/Drive Thru Event Gathering Use (MUDD-O, 3.28 acres)	270 Dwelling Units 2,500 SF 4,300 SF (No data)	1,960	Site Plan: 01-30-23
Proposed Zoning	Multifamily (Mid-Rise) Retail General Office or Single Family Attached Retail General Office (MUDD-O, 3.28 acres)	250 Dwelling Units 3,500 SF 4,300 SF or 250 Dwelling Units 3,500 SF 4,300 SF	1,598 or 2,310	Site Plan: 03-11-24

Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

Outstanding Issues

Strikethrough = Resolved

~~1. Curblines:~~

- ~~a. West Boulevard: Future back of curb location is 30' from the centerline with a typical section of a 4+ avenue with a shared use path.~~

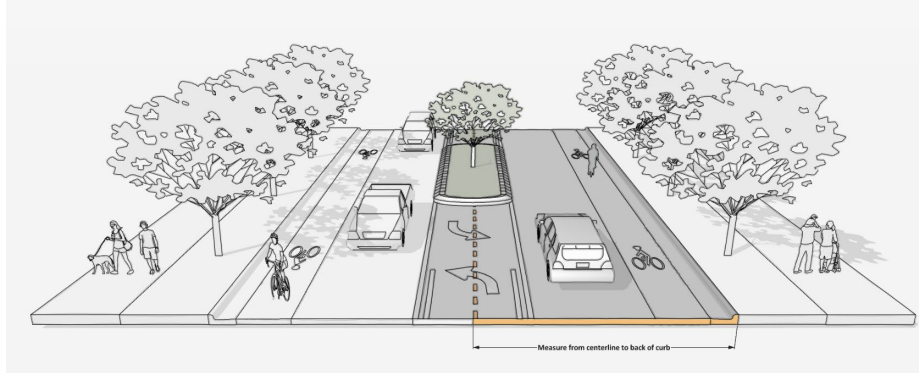


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- b. **South Mint Street:** Future back of curb is in its existing location



- c. **West Kingston Avenue:** Future back of curb is in its existing location

NOTE: Additional distance from center line to back of curb may be required in locations where a median is present or around intersections and interchanges.

Label and dimension the future curb and gutter location from the centerline for each road on the site plan.

2. **Traffic Study:**

~~A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating less than 2,500 daily trips and/or not triggering any other City TIS requirements.~~

3. ~~Under future UDO guidelines this project would require a Comprehensive Transportation Review and mitigation. In lieu of providing CTR, CDOT REQUESTS the petitioner provide APS pushbuttons and pedestrian ramp upgrades at the existing traffic signal at the intersection of West Boulevard and South Mint Street.~~

4. ~~Outstanding Comment from 7.26.23:~~ In coordination with NCDOT, revise site plan and conditional note(s) to commit to construct bicycle facilities by installing a 12' multi-use path on West Boulevard to meet the Council-adopted Charlotte BIKES Policy. Charlotte Streets Map calls for buffered bike lanes, NCDOT will not allow buffered bike lanes on West Boulevard. Multi-use paths maintained by the City shall meet ADA/PROWAG requirements.

~~Clarifying Comment from 11.27.23:~~ Proposed streetscape is only 8'. Due to NCDOT not wanting a bike lane on their street, this needs to be a 12' shared use path.

~~Clarifying Comment from 12.21.23:~~ Revise proposed streetscape to a 12' multi-use path to support council-adopted Charlotte BIKES policy.

5. ~~Outstanding Comment from 7.26.23:~~ Revise site plan to install the proposed streetscape off the future back of curb located 30' from the centerline.

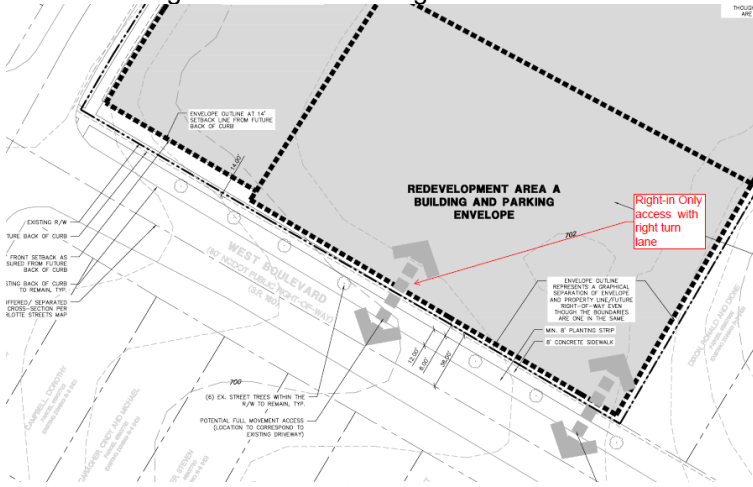
6. ~~Add a conditional note specifying "A Right of Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements."~~

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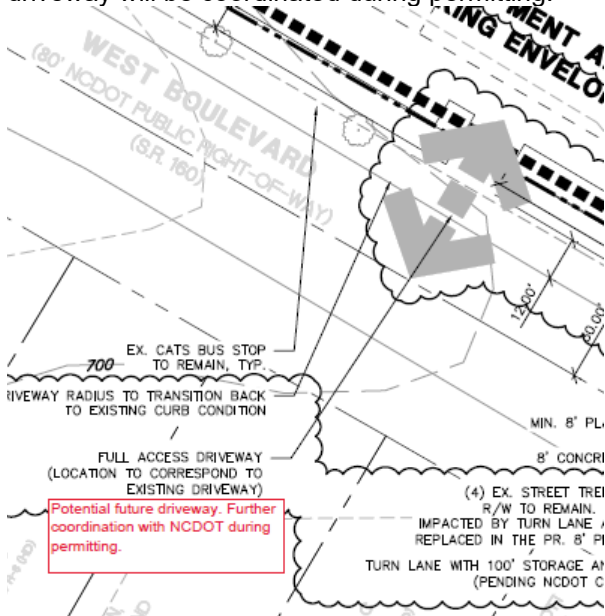
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7. ~~Outstanding Comment from 7.26.23:~~ Per coordination with NCDOT, the site access on West Boulevard needs to be right in ONLY with a right turn lane.



~~Clarifying Comment 11.27.23:~~ Revise site plane and conditional note to read, "Potential future driveway. Further coordination with NCDOT during permitting."

~~Clarifying Comment 12.20.23:~~ Although this is an existing driveway, NCDOT has the authority to remove or limit access. Add statement to callout and conditional note that approval of West Boulevard driveway will be coordinated during permitting.

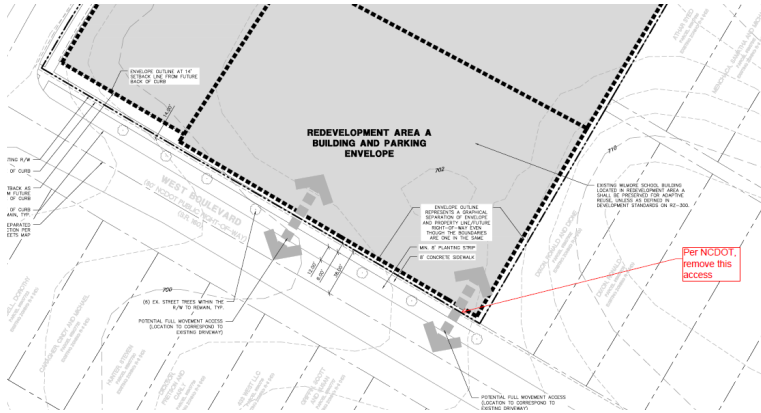


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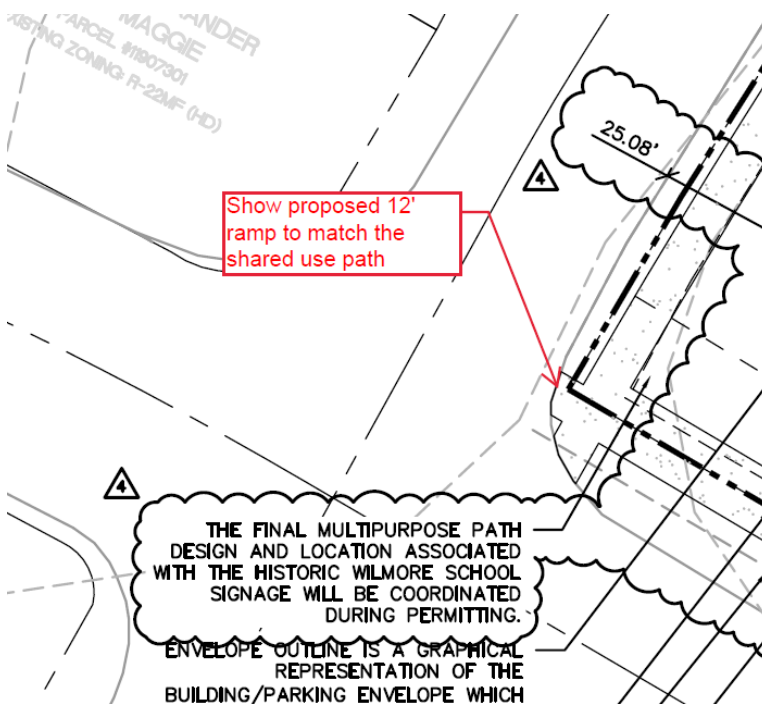
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8. ~~Outstanding Comment from 7.26.23:~~ Per coordination with NCDOT, remove this site access along West Boulevard.



9. ~~New Comment 3.20.24:~~ Show a proposed 12' ramp to match the shared use path width (ADA/PROWAG requirement).



10. ~~Add a conditional note specifying "All transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes."~~

11. ~~Add conditional note specifying "All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood that such improvements may be undertaken by the Petitioner on its~~

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own or in conjunction with other development or roadway projects taking place within the broad western Mecklenburg area, by way of a private/public partnership effort or other public sector project support.”

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>