

Rezoning Transportation Analysis

Petition Number: #2023-020

General Location Identifier: 12305403, 12305402

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Revision Log:

Date	Description
6-28-23	First Review (PDM)

General Review Information

The petition is located at the intersection of Morehead Street, a State-maintained major throughfare, and Myrtle Avenue, a City-maintained local street. The petition is located in a south corridor inside of Route 4, within the Center City 2020 Vision Plan.

Active Projects Near the Site:

- o N/A

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located at the intersection of Morehead Street, a State-maintained major throughfare, and Myrtle Avenue, a City-maintained local street. A Traffic Impact Study (TIS) is not required for this site. The proposed site plan will generate less vehicular trips than what is currently entitled under the existing zoning. Site plan and/or conditional note revisions are needed to commit to dimension and label all rights-of-way and future back of curb, committing to the Charlotte Streets map and CDLSM typical sections, and including the CDOT standard notes. Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Medical-Dental Office Building	38,557 SF	1,550	Tax Record
Entitled Use	Retail (B-1, 3.14 acres)	31,400 SF	1,555	General Guidance from Planning
Proposed Use	Multifamily (High-Rise) Retail (MUDD-O, 3.139 acres)	450 Dwelling Units 15,000 SF	1,715	Site Plan: 01-30-23

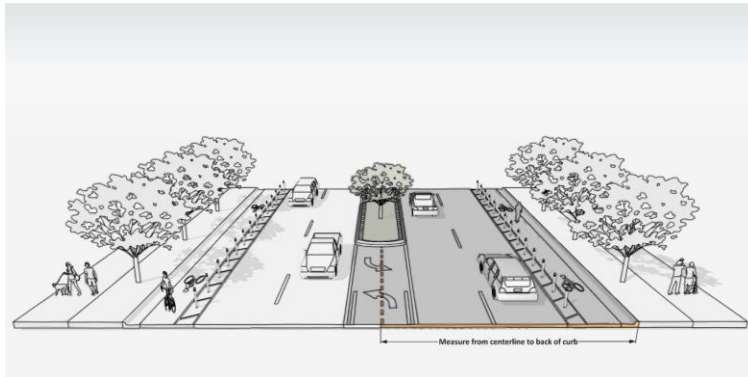
Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

Outstanding Issues

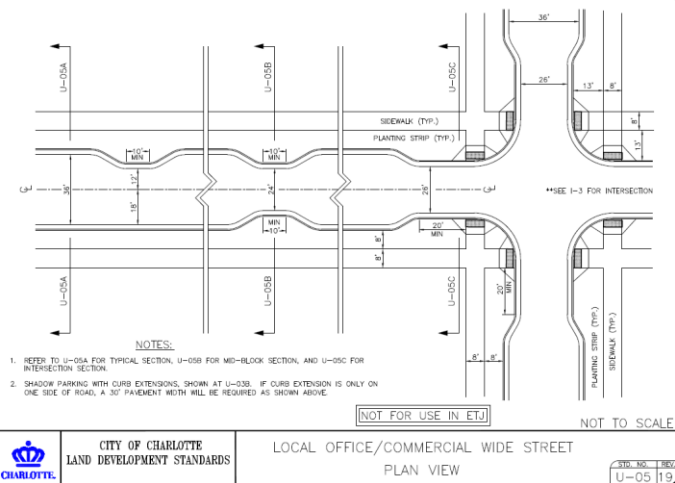
Strikethrough = Resolved

1. Curblines:

- Morehead Street:** Location of curb and gutter to be moved to 38 feet from roadway centerline to accommodate the Charlotte Streets Map with a roadway typical section of a four plus avenue with buffered bike lanes.



- Myrtle Avenue:** Location of curb and gutter to be moved to 20.5 feet from roadway centerline to accommodate the local commercial wide street typical section with on street parking (CLDSM U-05 Series).



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3. Comprehensive Transportation Review (CTR):

The Comprehensive Transportation Review was recently approved by City Council under the new UDO. CDOT has requested the petitioner complete a CTR for the site. The petition's zoning falls within the medium to high intensity development which based on the 1,455 daily trips will trigger Tier 2 (9 mitigation points) for multimodal assessment and Tier 2 (4 mitigation points) for transportation demand management assessment.

The petitioner shall review and assess the publicly accessible pedestrian network within ¼ mile walking distance of the site to identify multimodal infrastructure to meet the Tier 2 (9 mitigation points) for multimodal assessment. Petitioner shall also provide transportation demand management strategies to meet Tier 2 (4 mitigation points) to reduce vehicle trips and encourage alternative modes of transportation.

4. Revise site plan and conditional note(s) to commit to dedicate 40 feet of right-of-way from the road centerline of Morehead Street and 30 feet of right-of-way from the road centerline of Euclid Avenue if it does not exist. The site plan shall label and dimension the right-of-way from the road centerline.
5. Revise site plan and conditional note(s) to commit to construct an 8-foot planting strip, 8-foot sidewalk on all public road frontages. The site plan shall label and dimension both items from the back of curb and gutter or edge of pavement.
6. Revise site plan and conditional notes to replace all nonstandard curb on public road frontages with 2ft 6in curb and gutter.
7. Revise site plan and conditional note(s) to commit to construct bicycle facilities by locating the curb and gutter 38' from the center line on Morehead Street to meet the Council-adopted Charlotte BIKES Policy. Multi-use paths maintained by the City shall meet ADA/PROWAG requirements.
8. CDOT REQUESTS the petitioner provide a traffic signal at the intersection of Morehead Street and Myrtle Avenue.
9. Add a conditional note specifying "A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements."
10. Add a conditional note specifying "the Petitioner shall dedicate and convey in fee simple all rights-of-way to the City before the site's first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible." Provide SUE at 2ft behind sidewalk if located outside of the ROW.
11. Add a conditional note specifying "All transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes."
12. Add conditional note specifying "All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or roadway projects taking place within the broad Mecklenburg area, by way of a private/public partnership effort or other public sector project support."

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Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>