

Rezoning Transportation Analysis

Petition Number: 2022-176

General Location Identifier: 14909432

From: Jake Carpenter, PE

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Reviewer:

Patrick Monroe

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704-301-1411

Revision Log:

Date	Description
3-23-23	First Review (PDM)

General Review Information

The petition is located on the south side of Scaleybark Road, a City-maintained minor throughfare and west of Harford Avenue, a City-maintained local street. The petition is located in a south wedge inside of Route 4, within the Scaleybark Transit Station Area Plan.

Active Projects Near the Site:

- N/A

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located on the south side of Scaleybark Road, a City-maintained minor throughfare and west of Harford Avenue, a City-maintained local street. A Traffic Impact Study (TIS) may be required/not required for this site if the site generates more than 2,500 daily trips and/or triggers other City TIS requirements. Site plan and/or conditional note revisions are needed clarify the proposed land uses and densities, commit to removing the existing school flashers, and including the standard CDOT rezoning notes. Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Elementary School	814 Students	1,550	<i>Tax Record</i>
Entitlement with Current Zoning	Single Family (R-4, 15 acres)	60 Dwelling Units	635	<i>General Guidance from Planning</i>
Proposed Zoning	TOD-NC, 15 acres	-	Too many uses to determine trip generation	<i>Site Plan: 08-16-22</i>

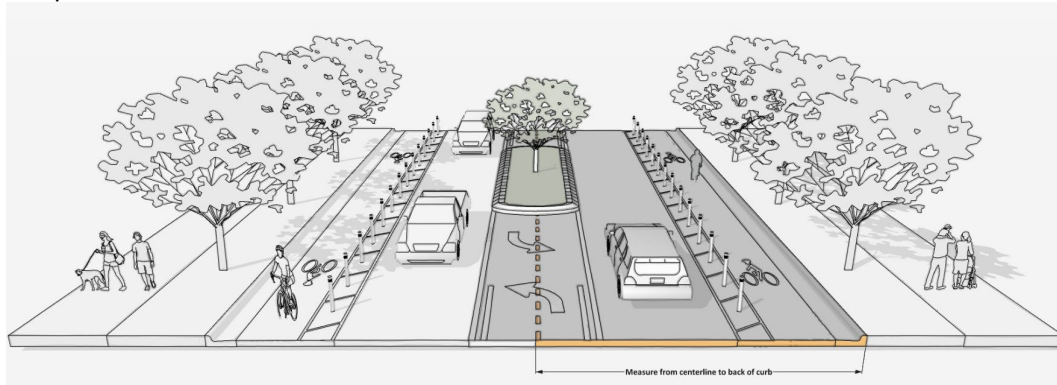
Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

Outstanding Issues

Strikethrough = Resolved

1. Curblines:

- a. **Scaleybark Road:** Location of the future curb and gutter is located 27 feet from roadway centerline to accommodate the Charlotte Streets Map with a roadway typical section of a two-plus avenue with buffered bike lanes. The existing curb line is not required to be relocated with this petition.



NOTE: Additional distance from center line to back of curb may be required in locations where a median is present or around intersections and interchanges.

Label and dimension the curb and gutter from the centerline for each road on the site plan.

2. Traffic Study:

A Traffic Impact Study may be required for the complete review of this petition if the site generating more than 2,500 daily trips and/or triggering other City TIS requirements. Land uses and densities for the proposed petition need to be clarified, so it can be determined if a TIS is required.

Staff will receive the petition and begin review, but the public hearing will not be scheduled until the TIS is approved to allow for the minimum time necessary for CDOT to review the study and reach agreement with the petitioner on the required transportation commitments. Any agreed upon commitments shall be included on the last site plan submitted no fewer than 4 weeks prior to the public hearing. Therefore, additional traffic mitigation comments may be forthcoming once the TIS is approved by CDOT (and NCDOT).

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3. Revise site plan and conditional notes to specify the proposed land uses and densities for the petition.
4. Revise site plan and conditional notes to show the horizontal street as a public street. In addition, provide CDLSM design standards for all proposed roads.
5. Revise site plan and conditional notes to commit to removing the existing school zone flashers and pavement markings. Include note that all signal equipment is to be returned to CDOT Operations Division. Further coordination is necessary to determine the plan for the existing HAWK signal in front of site once more information is provided on the land uses and densities.
6. Revise site plan and conditional note(s) to commit to dedicate 30-feet of right-of-way from the road centerline. The site plan shall label and dimension the right-of-way from the road centerline. If proposed sidewalk is outside of right-of-way provide additional right-of-way or provide sidewalk utility easement.
7. Add a conditional note specifying "A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements."
8. Add a conditional note specifying "the Petitioner shall dedicate and convey in fee simple all rights-of-way to the City before the site's first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible."
9. Add a conditional note specifying "All transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes."

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>