

Rezoning Transportation Analysis

Petition Number: #2022-161

General Location Identifier: 19906169A, 19906169B

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Reviewer: Patrick Monroe
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Revision Log:

Date	Description
02-23-23	First Review (PDM)
04-20-23	Second Review (PDM)
05-24-23	Third Review (DR)
07-26-23	Fourth Review (PDM)

General Review Information

The petition is located on the west side of Steele Creek Road, a State-maintained major throughfare south of Parkside Crossing Drive, a future City-maintained collector street. The petition is located in a Steele Creek Area Plan boundaries outside of Route 4.

Active Projects Near the Site:

- Steele Creek Rd/NC-160 Widening (Tryon St. to I-485)
 - U-5766B
 - Right-of-Way 2024

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The petition is located on the west side of Steele Creek Road, a State-maintained major throughfare south of Parkside Crossing Drive, a future City-maintained collector street. A Traffic Impact Study (TIS) was previously completed for this site under the previous rezoning petition, RZP 2019-128. The expected trip generation for this petition is comparable to what was previously approved in RZP 2019-128, and an updated traffic study is not required for this petition. All outstanding CDOT issues have been addressed.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	-	-	Tax Record
Entitlement with Current Zoning	Single Family (R-3, 29.33 acres)	87 Dwelling Units	890	General Guidance from Planning
Proposed Zoning	Single Family Attached (UR-2, 29.33 acres)	470 Dwelling Units	1,245	Site Plan: 08-18-22
Proposed Zoning	Single Family Attached (UR-2, 29.33 acres)	169 Dwelling Units	1,240	Site Plan: 04-10-23

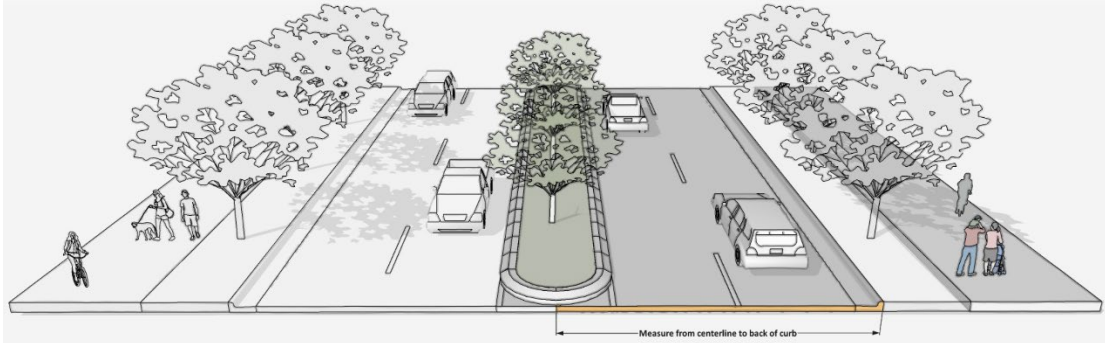
Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

Outstanding Issues

Strikethrough = Resolved

1. ~~Curbline:~~

- a. ~~**Steele Creek Road:** Location of the future curb and gutter to be installed at 33 feet from roadway centerline to accommodate the Charlotte Streets Map roadway typical section of four plus lane avenue with shared use path. The future curb and gutter should be shown on the plan and dimensioned from the centerline.~~



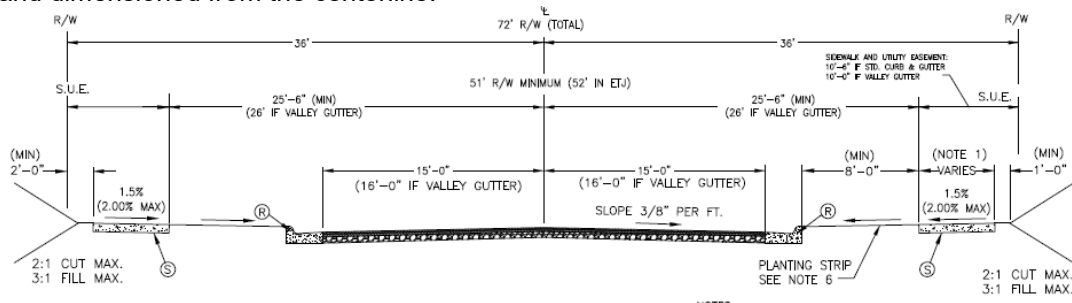
- b. ~~**Parkside Crossing:** Location of the future curb and gutter to be installed at 17.5 feet from roadway centerline to accommodate the Charlotte Streets Map roadway typical section of collector street per CLDSM U-07. The future curb and gutter should be shown on the plan~~

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and dimensioned from the centerline.



NOTE: Additional distance from center line to back of curb may be required in locations where a median is present or around intersections and interchanges.

Label and dimension the curb and gutter from the centerline for each road on the site plan.

2. Traffic Study:

~~Petitioner shall update previous submitted TIS (Gambrell Steel Creek Site) from August 2021 or provide documentation of land uses that show no update is needed.~~

~~Staff will receive the petition and begin review, but the public hearing will not be scheduled until the TIS is approved to allow for the minimum time necessary for CDOT to review the study and reach agreement with the petitioner on the required transportation commitments. Any agreed upon commitments shall be included on the last site plan submitted no fewer than 4 weeks prior to the public hearing. Therefore, additional traffic mitigation comments may be forthcoming once the TIS is approved by CDOT (and NCDOT).~~

~~**Update to Comment 4.20.2023:** A trip generation comparison was provided by the petitioner. The expected trip generation is comparable to what was previously approved, and an updated traffic study will not be required by CDOT.~~

~~3. Revise site plan and conditional note(s) to commit to dedicate all necessary ROW and easements along Steele Creek Road for the NCDOT U-5766B project.~~

~~**Update to Comment 4.20.2023:** Steele Creek Road is a major thoroughfare and requires a minimum of 50ft of ROW dedication.~~

~~4. Revise site plan and conditional note(s) to show the previously approved sidewalk and the curb and gutter along Parkside Crossing.~~

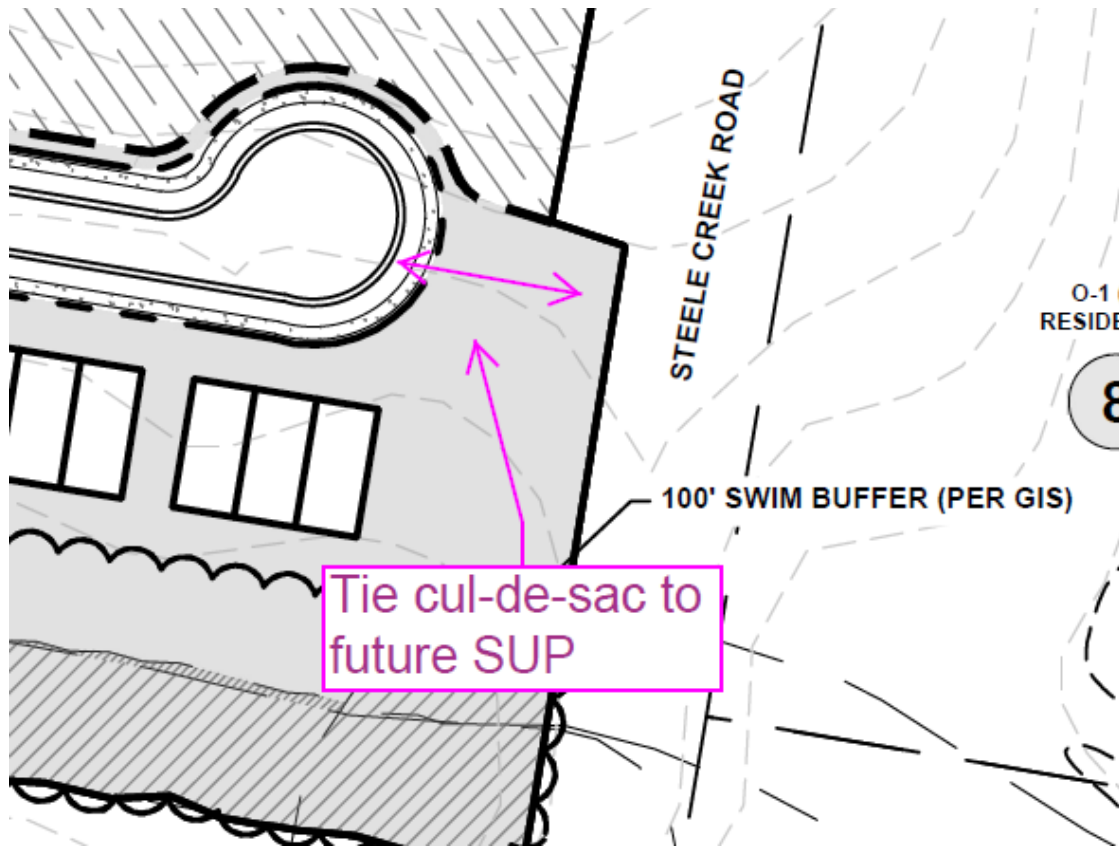
~~5. Revise site plan and conditional note(s) to commit to construct bicycle facilities by installing a 12' multi-use path to meet the Council adopted Charlotte BIKES Policy. Multi-use paths maintained by the City shall meet ADA/PROWAG requirements.~~

~~6. CDOT REQUEST: Revise site plan and conditional notes to commit to providing a 12-ft multi-use path connection from the proposed cul-de-sac to the proposed MUP along Steele Creek~~

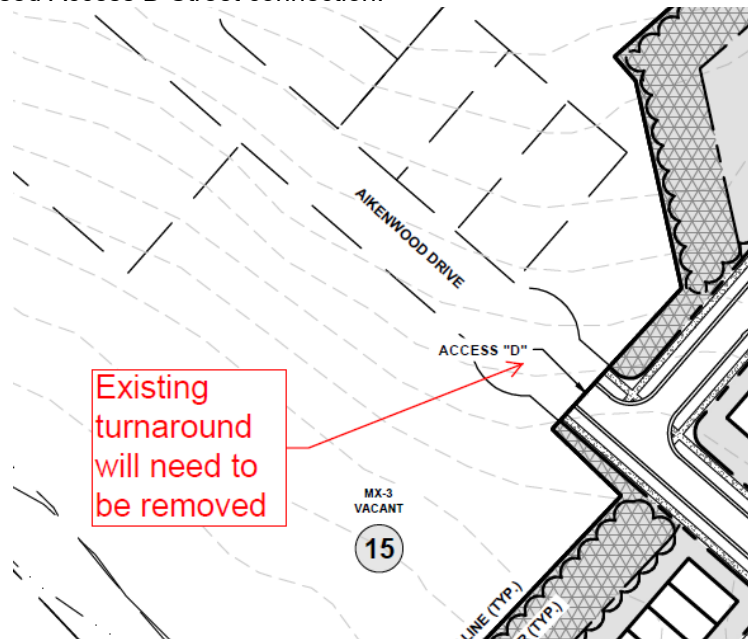
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7. ~~Revise site plan and conditional notes to commit to removing the existing turnaround on Aikenwood Drive at the proposed Access D Street connection.~~



~~Update to Comment 4.20.2023: Show proposed road extension tying into Access A.~~

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- ~~8. **Outstanding Comment from 02-23-2023:** Add a conditional note specifying "A Right of Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements."~~
- ~~9. **Outstanding Comment from 02-23-2023:** Add conditional note specifying "All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or roadway projects taking place within the broad southwestern Mecklenburg area, by way of a private/public partnership effort or other public sector project support."~~

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>