

Rezoning Transportation Analysis

Petition Number: 2022-159

General Location Identifier: 19931104, 19931103

From: Jake Carpenter, PE

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Reviewer:

Patrick Monroe

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Revision Log:

Date	Description
02-23-23	First Review (JLP)

General Review Information

The petition is located on the north side of York Road, a State-maintained major throughfare west of Majestic Oak Drive, a State-maintained minor collector. The petition is located in a Lake Wylie wedge outside of Route 4, within the Steele Creek Area Plan.

Active Projects Near the Site:

- N/A

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The petition is located on the north side of York Road, a State-maintained major throughfare west of Majestic Oak Drive, a State-maintained minor collector. A Traffic Impact Study (TIS) is not required for this site due to the site generating less than 2,500 trips. Site plan and/or conditional note revisions are needed to commit to installing a MUP along York Road in accordance with the Charlotte BIKES policy, install right turn lane on York Road at the proposed site access, and including the CDOT standard notes. Further details are listed below.

Rezoning Transportation Analysis

Petition Number: 2022-159

General Location Identifier: 19931104, 19931103

Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	-	-	<i>Tax Record</i>
Entitlement with Current Zoning	Single Family (R-3, 10.788 acres)	32 Dwelling Units	355	<i>General Guidance from Planning</i>
Proposed Zoning	Single Family Attached (R-8MF, 10.788 acres)	70 Dwelling Units	485	<i>Site Plan: 07-19-22</i>

Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

Outstanding Issues

Strikethrough = Resolved

1. Curbside:

- York Road:** Location of the future curb and gutter to be installed at 33 feet from roadway centerline to accommodate the Charlotte Streets Map roadway typical section of four lane avenue with shared use path. The future curb and gutter should be shown on the plan and dimensioned from the centerline.



Label and dimension the curb and gutter from the centerline for each road on the site plan.

2. Traffic Study:

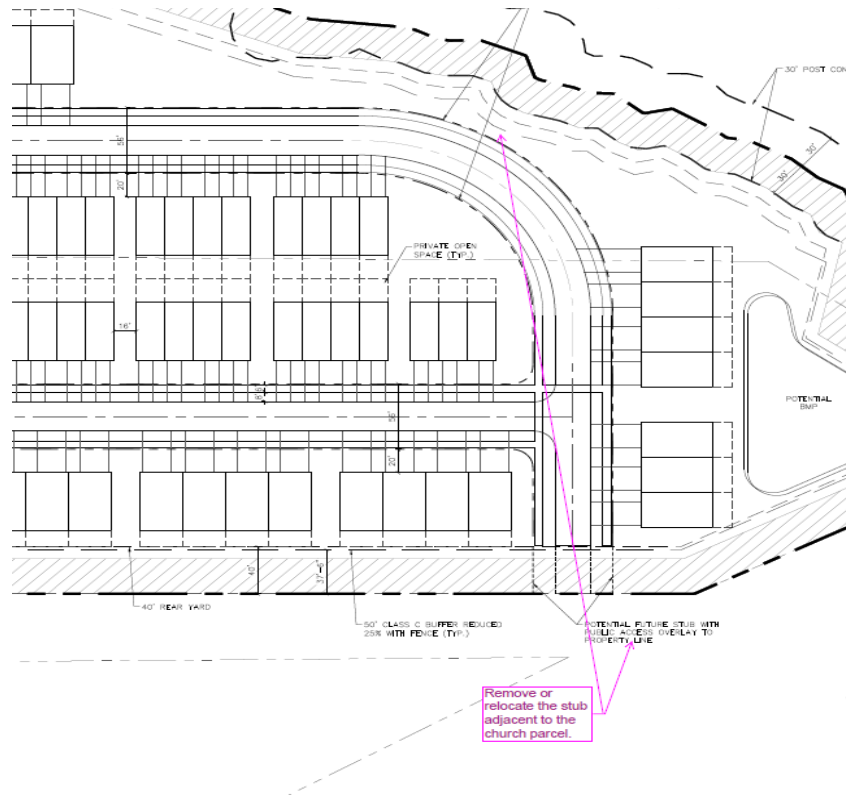
A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating less than 2,500 daily trips.

- Revise site plan and conditional note(s) to commit to dedicate 50' right-of-way from the road centerline along York Road. The site plan shall label and dimension the right-of-way from the road centerline.
- Revise site plan to callout all internal streets as private roads with public access easements.
- Revise site plan to remove the stub to the adjacent park property. If required by Subdivision the stub should be provided to the property to the west and not the park property.

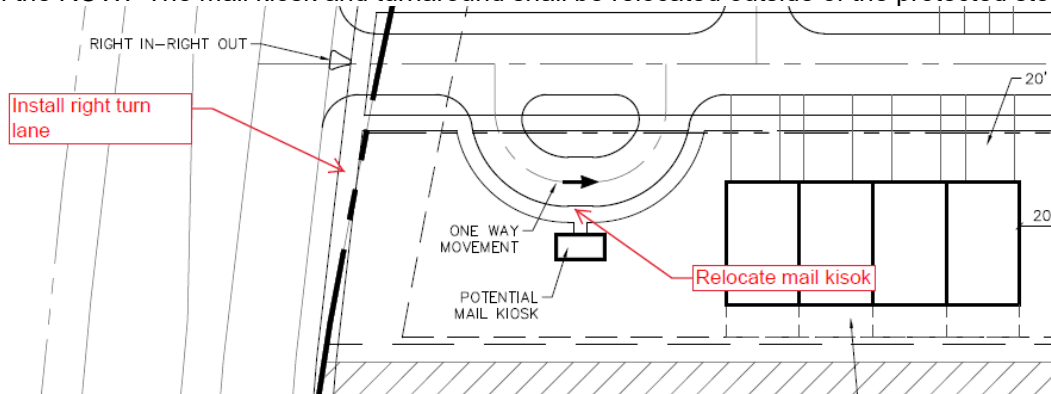
Rezoning Transportation Analysis

Petition Number: 2022-159

General Location Identifier: 19931104, 19931103



6. Revise site plan and conditional note(s) to commit to construct bicycle facilities by installing a 8' planting strip and 12' multi-use path to meet the Council-adopted Charlotte BIKES Policy. Multi-use paths maintained by the City shall meet ADA/PROWAG requirements.
7. Per coordination with NCDOT, revise site plan and conditional note(s) to commit to installing a right turn lane into the site off York Road with the appropriate taper and storage.
8. Per coordination with NCDOT, revise site plan to provide a 100ft protected driveway stem measured from the ROW. The mail kiosk and turnaround shall be relocated outside of the protected stem.



9. Add a conditional note specifying "the Petitioner shall dedicate and convey in fee simple all rights-of-way to the City before the site's first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible."
10. Add conditional note specifying "All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road

Rezoning Transportation Analysis

Petition Number: 2022-159

General Location Identifier: 19931104, 19931103

system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or roadway projects taking place within the broad southwestern Mecklenburg area, by way of a private/public partnership effort or other public sector project support.”

11. If site is to remain in the ETJ: Revise site plan and conditional note(s) to construct 8-foot planting strips and 12-foot multi-use paths on York Road. The site plan shall label and dimension both items from the back of curb and gutter or edge of pavement. Add conditional note that the entire facility must be outside of the right of way and within a public access easement. Add conditional note committing to construction and maintenance of the 12-foot multi-use path on York Road.

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>