

Rezoning Transportation Analysis

Petition Number: 2022-155

General Location Identifier: 03715511

From: Jake Carpenter, PE

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Reviewer:

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Revision Log:

Date	Description
1-25-23	First Review (TM)
2-23-23	Second Review (TM)

General Review Information

The petition is located south of Lakeview Road, a State-maintained major throughfare west of Trinity Road, a City-maintained major collector. The petition is located in a northwest wedge outside of Route 4, within the Westside Strategy Plan Study Area.

Active Projects Near the Site:

- Lakeview Road/Reames Road Roundabout and Sidewalk
 - This project will improve the intersection of Lakeview Road and Reames Road with a roundabout and includes installation of sidewalk on the south side of Lakeview Road from Reames Road to Beatties Ford Road.
 - Currently in construction

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The petition is located south of Lakeview Road, a State-maintained major throughfare west of Trinity Road, a City-maintained major collector. A Traffic Impact Study (TIS) is not required for this site due to the site generating less than 2500 daily trips. Site plan and/or conditional note revisions are needed to meet standard CDOT conditional note requirements. Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	-	-	<i>Tax Record</i>
Entitlement with Current Zoning	Single Family (R-4, 4.27 acres)	17 Dwelling Units	200	<i>General Guidance from Planning</i>
Proposed Zoning	Single Family Attached (R-12MF, 4.27 acres)	40 Dwelling Units	255	<i>Site Plan: 08-8-22</i>
Proposed Zoning	Single Family Attached (R-12MF, 4.27 acres)	41 Dwelling Units	255	<i>Site Plan: 02-13-23</i>

Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

Outstanding Issues

Strikethrough = Resolved

1. ~~Curbline:~~

- a. ~~Lakeview Road:~~ Location of future curb and gutter to be moved to 30 feet from roadway centerline to accommodate the Charlotte Streets Map with a future roadway typical section of a 4+ Avenue (including a two-way left turn lane) and Shared Use Path.

~~NOTE: Additional distance from center line to back of curb may be required in locations where a median is present or around intersections and interchanges.~~

~~Label and dimension the curb and gutter from the centerline for each road on the site plan.~~

2. ~~Traffic Study:~~

~~A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating less than 2,500 daily trips and/or not triggering any other City TIS requirements.~~

3. ~~Revise site plan and conditional note(s) to commit to dedicate 28 feet of right of way from the road centerline of Roadway A. The site plan shall label and dimension the right of way from the road centerline.~~

4. ~~Revise site plan and conditional note(s) to commit to construct an 8-foot planting strip, and 6-foot sidewalk on Road A per Chapter 19. The site plan shall label and dimension both items from the back of curb and gutter or edge of pavement.~~

5. **Outstanding Comment from 01-25-23:** Add a conditional note specifying "A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements."

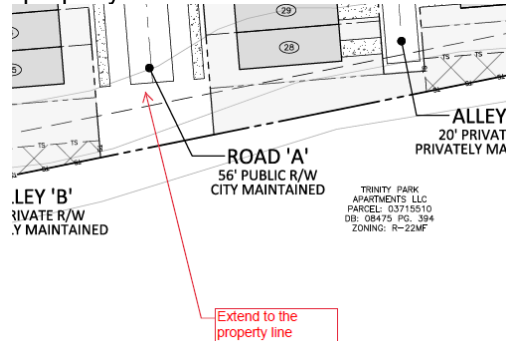
6. ~~Revise site plan and conditional note(s) to commit to completing a developer agreement with the active CIP project prior to permitting approval. Revise site plan to show the updated easement information and CIP project linework including streetscape and ROW that has been previously purchased.~~

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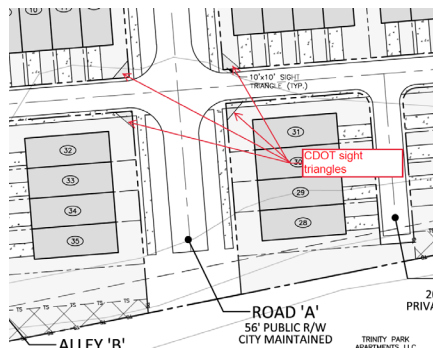
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7. ~~Revise site plan and conditional note(s) to commit to extending road A and roadway infrastructure (curb line and sidewalk) to the property line.~~



8. ~~Revise site plan and conditional note(s) to commit to clearly identifying Road A as standard from the CLDSM. Add standard to the callout of road A as well as add a conditional note committing to constructing roadway A to a CLDSM standard. Site plan shall label and dimension roadway infrastructure from the roadway centerline including ROW.~~
9. ~~Revise site plan and conditional note(s) to commit to clearly identifying Alley cross sections as a standard from the CLDSM. Add standard to the callout of alleyways as well as add a conditional note committing to constructing them to CLDSM standard.~~
10. ~~Revise site plan by dimensioning the sidewalk along Alley A.~~
11. ~~Revise site plan by adding CDOT sight triangles to the intersection of Alley A and Roadway A per CDOT sight distance policy. Show either the 50 x 50 sight triangle or the 35 x 3 sight triangle.~~



12. ~~Add a conditional note specifying "the Petitioner shall dedicate and convey in fee simple all rights-of-way to the City before the site's first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible."~~
13. ~~Add a conditional note specifying "All transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes."~~
14. ~~Add conditional note specifying "All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or roadway projects taking place within the broad~~

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~~northwestern Mecklenburg area, by way of a private/public partnership effort or other public sector project support."~~

15. CDOT NOTE 02-22-23: Driveways will be required to be moved out of sight triangles.

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>