

# Rezoning Transportation Analysis

Petition Number: 2022-131

General Location Identifier: 05312213, 05312214, 05312219, 05312220, 05312224, 05312228, 05313103, 05313114, 05313206

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**Reviewer:** Patrick Monroe  
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Revision Log:	Date	Description
	12-29-22	First Review (PDM)

## General Review Information

The petition is located on the west side of Moores Chapel Road, a State-maintained minor throughfare south of Charlie Hipp Road, a State-maintained minor collector. The petition is located in a northwest wedge outside of Route 4, and within the Catawba Area Plan.

Active Projects Near the Site:

- N/A

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*CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.*

*This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.*

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Based on our review of the petition, we offer the following information for your consideration.

## Transportation Summary

The petition is located on the west side of Moores Chapel Road, a State-maintained minor throughfare south of Charlie Hipp Road, a State-maintained minor collector. A Traffic Impact Study (TIS) is not required for this site. Site plan and/or conditional note revisions are needed to label and dimension the back of curb and right-of-way along each road, revise site access per CDOT and NCDOT access guidelines, amending the alignment of Performance Road Extension, and including the CDOT standard notes. Further details are listed below.

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## Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Single Family	4 Dwelling Units	40	Tax Record
Entitlement with Current Zoning	Single Family (R-3, 35.26 acres)	105 Dwelling Units	1,060	General Guidance from Planning
Proposed Zoning	Single Family Attached (R-8MF, 35.26 acres)	170 Dwelling Units	1,245	Site Plan: 07-7-22

**Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.**

## Outstanding Issues

**Strikethrough = Resolved**

### 1. Curblines:

a. **Moore's Chapel Road:** Location of curb and gutter to be installed 19 feet from roadway centerline to accommodate the Charlotte Streets Map with a roadway typical section of a 2+ Avenue with shared use path.

**Performance Road:** Location of curb and gutter to be installed 24 feet from roadway centerline to accommodate the Charlotte Streets Map with a roadway typical section of a 2+ Avenue with bike lane.

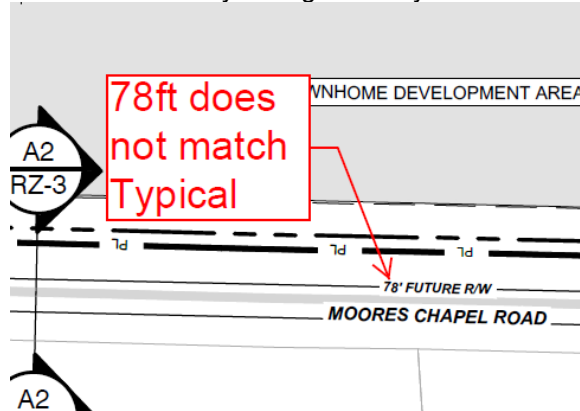
NOTE: Additional distance from center line to back of curb may be required in locations where a median is present or around intersections and interchanges.

Show, label, and dimension the curb and gutter from the centerline for each road on the site plan.

### 2. Traffic Study:

A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating less than 2,500 daily trips and/or not triggering any other City TIS requirements.

3. Revise site plan to dimension and label the existing and proposed right-of-way on all public streets. Reconcile site plan or typical section to clarify the right-of-way on Moore's Chapel Road.

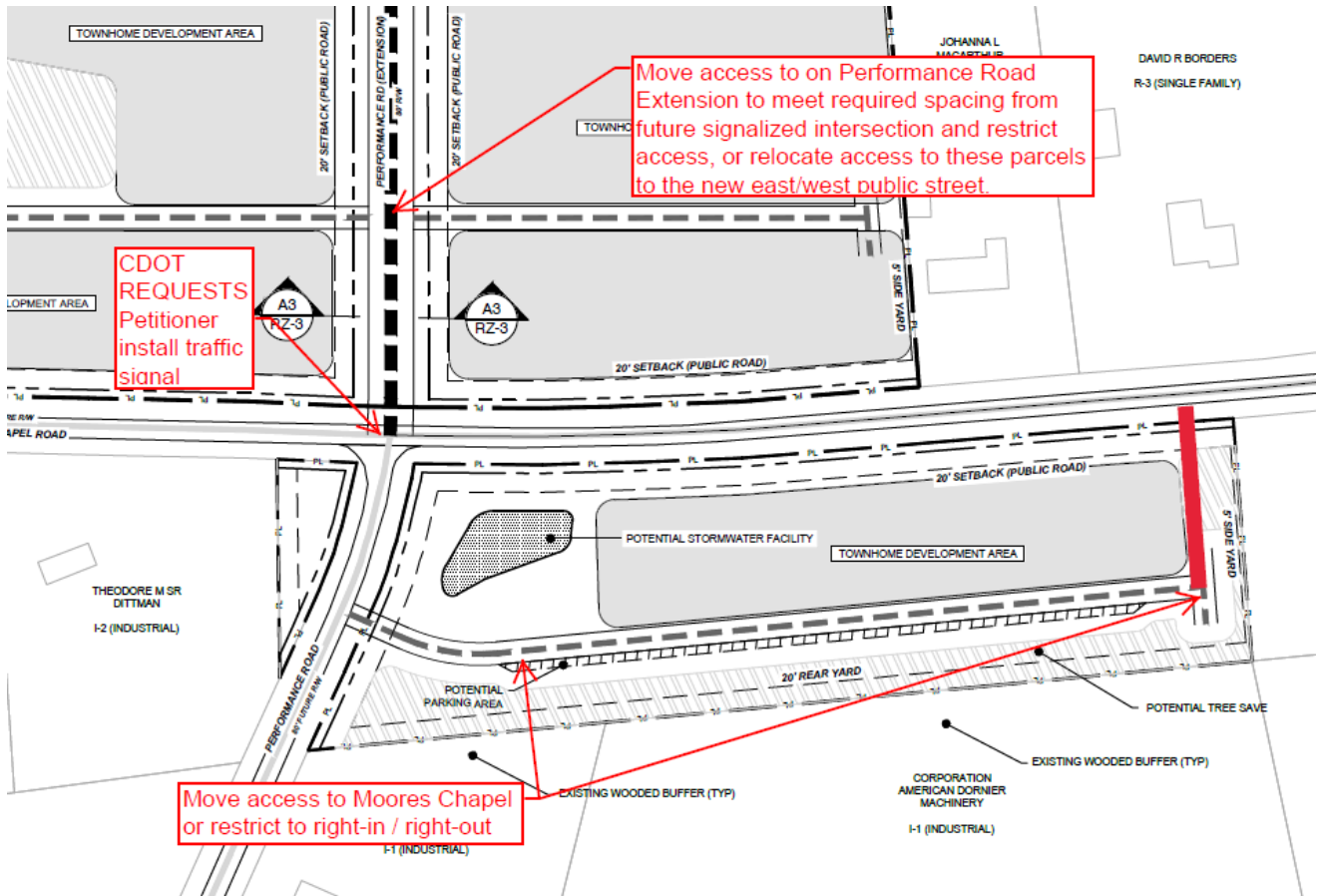


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4. The proposed alignment of Performance Road extension does not match the approved CTP alignment of the road. Revise site plan to align Performance Road with the approved CTP alignment. If CTP alignment is to be amended, further coordination is necessary with CDOT.
5. Per coordination with NCDOT, revise site plan and conditional notes to provide an eastbound left turn lane and westbound right and left turn lanes on Moores Chapel Road at Performance Road with appropriate storage length and taper per NCDOT standards.
6. Per coordination with NCDOT, revise site plan and conditional notes to the proposed sight access per the markup below. CDOT and NCDOT anticipate a future traffic signal will be needed at the intersection of Moores Chapel Road and Performance Road, so access spacing / restriction requirements should be considered in relation to the future traffic signal.



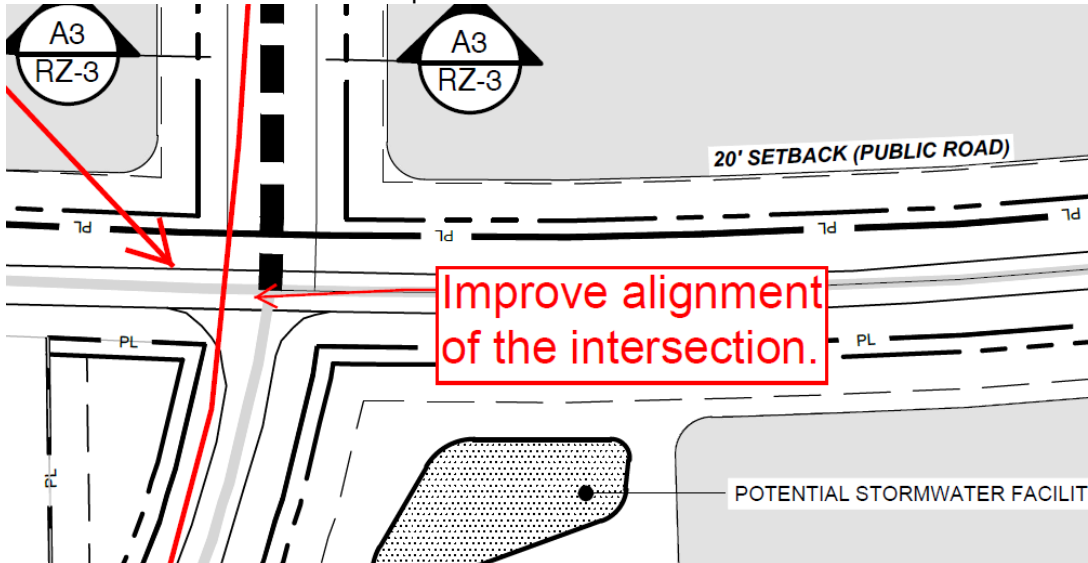
7. CDOT REQUESTS the petitioner revise site plan and conditional notes to provide a traffic signal at the intersection of Moores Chapel Road and Performance Road.

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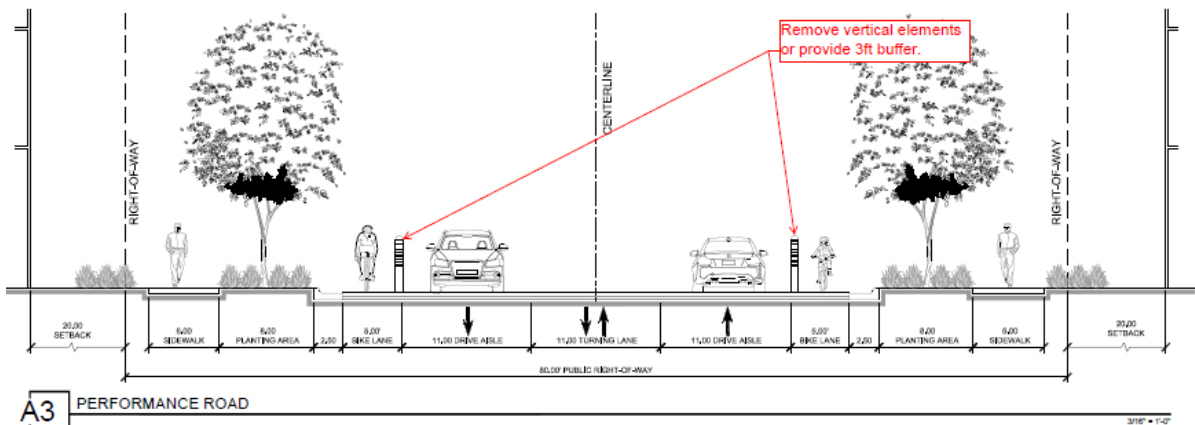
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- Per coordination with NCDOT revise site plan and conditional notes to improve the alignment of Performance Road at Moores Chapel Road.



- Revise site plan typical section of Performance Road to remove the vertical elements from the bike lane. If vertical elements are proposed, provide a 3ft buffer between the bike lane and vehicle lane.



- Revise site plan to provide a north arrow and scale.
- Add a conditional note specifying "All transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes."
- Add conditional note specifying "All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or roadway projects taking place within the broad western Mecklenburg area, by way of a private/public partnership effort or other public sector project support."
- Revise site plan to show all required sidewalk facilities on the plan. If petition will remain in ETJ, sidewalk facilities along existing and proposed NCDOT maintained roads should be located such that the entire sidewalk facility is outside of the right of way and within a public access easement. Add

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conditional note committing to construction and maintenance of the X-foot sidewalks/multi-use paths on X Road.

14. Add a conditional note specifying “the Petitioner shall dedicate and convey in fee simple all rights-of-way to the City before the site’s first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2’ behind back of sidewalk where feasible.”

## Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>