

Rezoning Transportation Analysis

Petition Number: #2022-107

General Location Identifier: 08119159

From: Jake Carpenter, PE

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Reviewer:

Isaiah Washington

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Revision Log:

Date	Description
11-28-22	First Review (IW)
12-22-22	Second Review (DR)
01-26-23	Third Review (IW)
03-22-23	Fourth Review (IW)
11-27-23	Fifth Review (ME)
1-24-24	Sixth Review (ME)
2-27-24	Seventh Review (ME)

General Review Information

The petition is located on the south side of Parkwood Avenue, a City-maintained major throughfare east of Hawthorne Lane, a City-maintained minor throughfare. The petition is located in an east wedge inside of Route 4, and within the Center City 2020 Vision Plan.

Active Projects Near the Site:

- No active projects near site

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The petition is located on the south side of Parkwood Avenue, a City-maintained major throughfare east of Hawthorne Lane, a City-maintained minor throughfare. A Traffic Impact Study (TIS) is not needed for this. All CDOT comments have been addressed.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	-	-	Tax Record
Entitlement with Current Zoning	Single Family Multifamily (R-22MF & R-5, 1 acre)	3 Dwelling Units 6 Dwelling Units	50	General Guidance from Planning
Proposed Zoning	Single Family Attached Multifamily (Low-Rise) (UR-2, 1 acre)	3 Dwelling Units 22 Dwelling Units	245	Site Plan: 6/10/2022
Proposed Zoning	Single Family Attached (UR-2, 1 acre)	18 Dwelling Units	87	Site Plan: 6/10/2022

Provide comments to the specified comments below.

Outstanding Issues

Strikethrough = Resolved

1. ~~**Curblin**~~: The proposed zoning district has a setback measured from back of the existing or proposed future curblin.

a. ~~**Parkwood Avenue**~~: Location of future back of curb and gutter is 38 feet from centerline.

Clarified Outstanding Comment from 11-28-22 Label and dimension the existing and future curb and gutter from the centerline for each road on the site plan. Streetscape elements should be constructed at the back of future curb and gutter.

Label and dimension existing bike lane along Parkwood from centerline. Label and dimension proposed streetscape from centerline.

2. ~~**Traffic Study: (Choose One)**~~

A Traffic Impact Study (TIS) is not necessary for the complete review of this petition due to the site generating less than 2,500 daily trips. If during the permitting process the site generates more than 2,500 daily trips, then a traffic study will be required.

3. ~~**Outstanding Comment from 11-28-22**~~ Revise site plan and conditional note(s) to commit to dedicate 56 feet of right of way from the road centerline. The site plan shall label and dimension the right of way from the road centerline.

Clarified Outstanding Comment from 03-21-22 Site plan only shows hatched area, petitioner should label and dimension the right of way from the centerline for each road on the site plan.

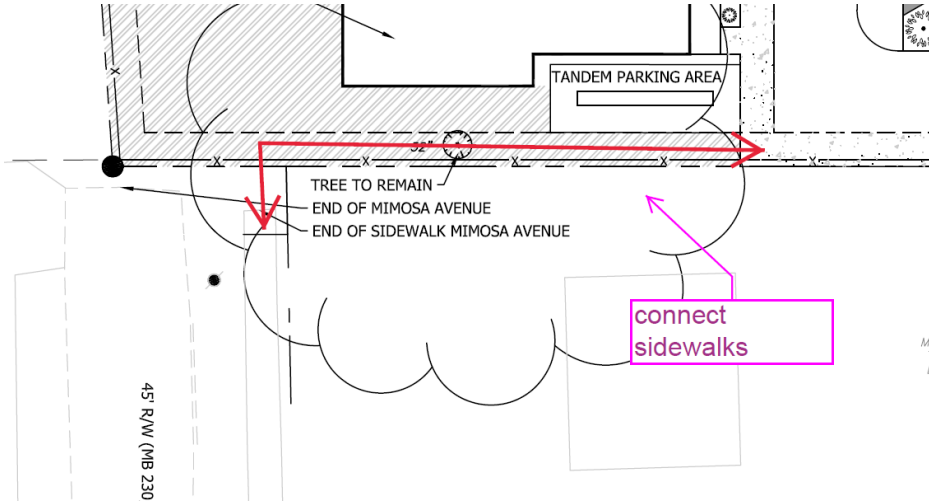
4. ~~**Comment Rescinded 3-22-23:**~~ Remove “and pedestrian” from conditional note 2 under Transportation, CDOT requests pedestrian access between Mimosa Avenue and the proposed site.

5. ~~**Comment Rescinded 3-22-23:**~~ CDOT requests the site plan and conditional note(s) revisions to commit to connect sidewalk near single family structure to existing sidewalk on Mimosa Avenue.

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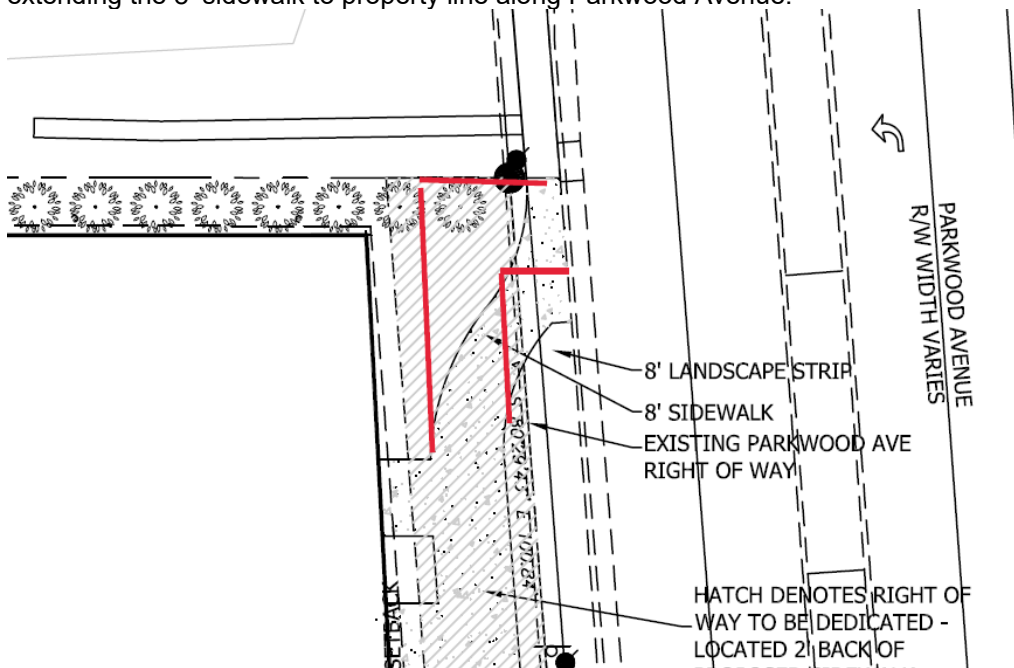
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Clarifying comment from 12/22/2022: Commit to connect sidewalk near single family structure to existing sidewalk on Mimosa Avenue as shown above.

- 6. ~~Comment rescinded 03-23-23~~ Site plan and conditional note(s) revisions are needed to commit to extending the 8' sidewalk to property line along Parkwood Avenue.



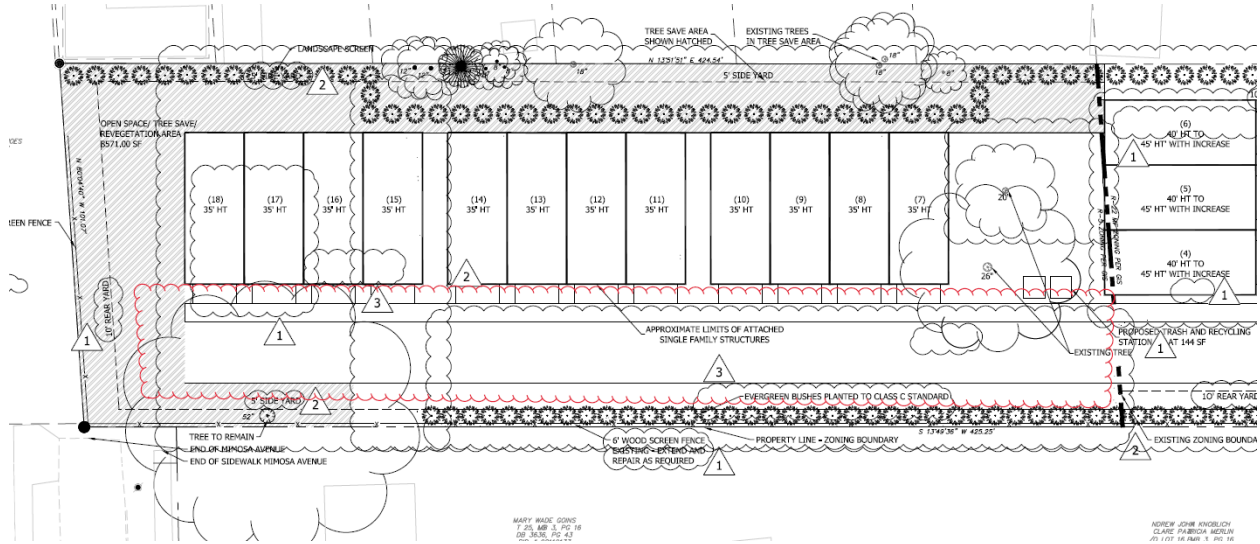
Clarifying comment from 12/22/2022: Extend 8-foot sidewalk as shown above to property line along Parkwood Avenue.

- 7. ~~New Comment 11-27-23~~ Revise site plan to accommodate Fire and solid waste vehicle turnaround requirements.

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Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. The setback for this district is measured from the back of the existing or future curbline as determined by CDOT and Planning during the permitting process.
2. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
3. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
4. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
5. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
6. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
7. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
8. The petitioner needs to complete and submit the [Right of way Abandonment Petition form](#) to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible

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abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.

9. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:

<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>