

# Rezoning Transportation Analysis

Petition Number: #2022-099

General Location Identifier: 12901301

From: Jake Carpenter, PE

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Reviewer:

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## Revision Log:

Date	Description
11-28-22	First Review (IW)
12-22-22	Second Review (IW)
01-26-23	Third Review (IW)
10-25-23	Fourth Review (NK)

## General Review Information

The petition is located on the south side of Commonwealth Avenue, a City-maintained major throughfare east of The Plaza, a City-maintained local street. The petition is located in a southeast corridor inside of Route 4, and within the Plaza-Central Pedscape Plan.

## Active Projects Near the Site:

- LYNX Silver Line
  - Proposed LYNX Silver Line light project from the Town of Matthews through Southeast and West Charlotte to the City of Belmont.
  - Currently in Design

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CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

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Based on our review of the petition, we offer the following information for your consideration.

## Transportation Summary

The petition is located on the south side of Commonwealth Avenue, a City-maintained major collector east of The Plaza, a City-maintained local street. A Traffic Impact Study (TIS) is not needed for this site. The petition is committed to upgrading streetscape along The Plaza and Commonwealth Avenue and upgrading off-site ADA Ramps. No further revisions are required.

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## Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	-	-	Tax Record
Entitlement with Current Zoning	Office (O-2, 0.94 acres)	14,100 SF	215	General Guidance from Planning & RZP# 1989-035
Proposed Zoning	Multifamily (High-Rise) Retail (MUDD-O, 0.94 acres)	175 Dwelling Units 12,000 SF	1,775	Site Plan: 5/25/2022
Proposed Zoning	Multifamily (High-Rise) Retail Drive-In Bank (MUDD-O, 0.94 acres)	175 Dwelling Units 6,000 SF 6,000 SF	2,120	Site Plan: 10/19/2023

Provide comments to the specified comments below.

### Outstanding Issues

**Strikethrough = Resolved**

1. ~~**Curbline:** The proposed zoning district has a setback measured from back of the existing or proposed future curbline.~~

a. ~~**Commonwealth Avenue:** The future location of curb and gutter is in its existing location.~~

b. ~~**The Plaza:** The future location of curb and gutter is in its existing location~~

~~Label and dimension the curb and gutter from the centerline for each road on the site plan.~~

2. ~~**Traffic Study: (Choose One)**~~

~~A Traffic Impact Study (TIS) is not necessary for the complete review of this petition due to the site generating less than 2,500 daily trips. If during the permitting process the site generates more than 2,500 daily trips, then a traffic study will be required.~~

3. ~~**Outstanding Comment from 11-28-22: Comprehensive Transportation Review (CTR):**~~

~~The Comprehensive Transportation Review was recently approved by City Council under the new UDO. In lieu of a TTM for the proposed site CDOT has requested the petitioner complete a CTR for the site. The petition's zoning falls within the medium to high intensity development which based on the 1,775 daily trips will trigger Tier 3 (14 mitigation points) for multimodal assessment and Tier 3 (6 mitigation points) for transportation demand management assessment.~~

~~The petitioner shall review and assess the publicly accessible pedestrian network within ¼ mile walking distance of the site to identify multimodal infrastructure to meet the Tier 3 (14 mitigation points) for multimodal assessment. Petitioner shall also provide transportation demand management strategies to meet Tier 3 (6 mitigation points) to reduce vehicle trips and encourage alternative modes of transportation.~~

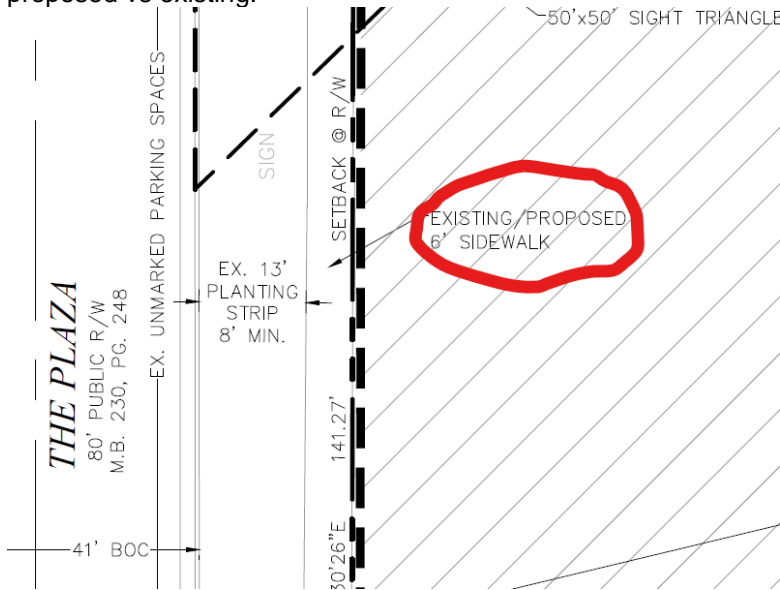
4. ~~Revise site plan and conditional note(s) to commit to dedicate 2 feet of sidewalk utility easement behind proposed sidewalk along Commonwealth Avenue and The Plaza. The site plan shall label and dimension sidewalk utility easement.~~

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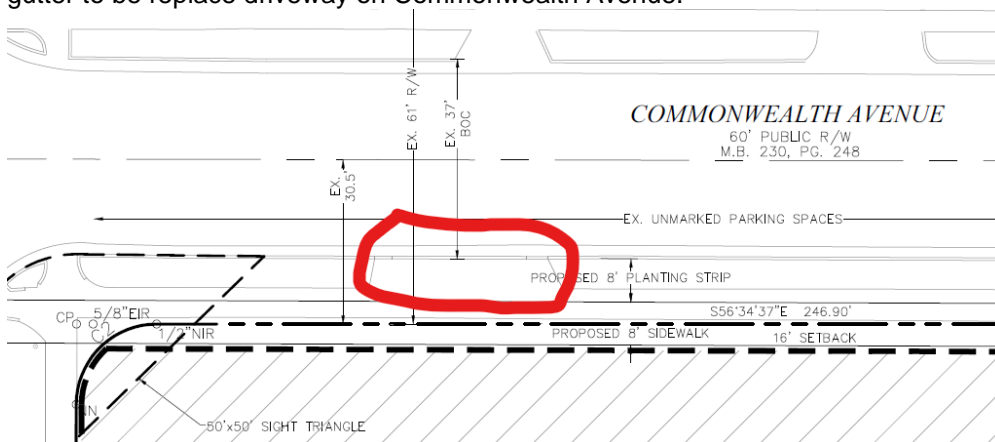
Petition Number: #2022-099

General Location Identifier: 12901301

5. ~~Revise site plan and conditional note(s) to specify what portion of the sidewalk along The Plaza will be proposed vs existing.~~



6. ~~Revise site plan and conditional note(s) to show existing western driveway to be removed and curb and gutter to be replace driveway on Commonwealth Avenue.~~



7. ~~CDOT is requesting that the petitioner revise the site plan and conditional notes to commit to updating the ADA ramps at corner of The Plaza and Commonwealth Avenue.~~
8. ~~A site plan note(s) specifying dedication and fee simple conveyance of all rights-of-way to the City before the site's first building certificate of occupancy is issued as needed. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible.~~

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Petition Number: #2022-099

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- ~~9. A site plan note specifying that all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued as needed. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.~~

## Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. The setback for this district is measured from the back of the existing or future curbline as determined by CDOT and Planning during the permitting process.
2. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
3. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
4. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
5. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
6. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
7. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
8. The petitioner needs to complete and submit the [Right of way Abandonment Petition form](#) to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.
9. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:  
<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>