

# Rezoning Transportation Analysis

Petition Number: #2022-048

General Location Identifier: 02911102, 02911103, 02911106, 02965106, 02965107, 02904108, 02965109, and 02903113

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## Revision Log:

| Date     | Description         |
|----------|---------------------|
| 08-25-22 | First Review (PDM)  |
| 09-22-22 | Second Review (PDM) |
| 10-27-22 | Third Review (TM)   |
| 11-28-22 | Fourth Review (TM)  |
| 01-25-23 | Fifth Review (DR)   |
| 04-06-23 | Sixth Review (TM)   |
| 04-25-23 | Seventh Review (TM) |
| 05-24-23 | Eight Review (DR)   |
| 06-28-23 | Ninth Review (DR)   |
| 07-26-23 | Tenth Review (DR)   |

## General Review Information

The petition is located adjacent to Galloway Road, a privately maintained local road and Mallard Glen Drive, a City maintained local road. The petition is located within the Northeast Corridor wedge and inside the I-485 Interchange Analysis area plan boundary. This petition is also located outside of Route 4.

### Active Projects Near the Site:

- I-85 & I-485 Interchange Ramp Improvements
  - TIP # I-6012
  - ROW 2028 (Awaiting Funding)

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*CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.*

*This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.*

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Based on our review of the petition, we offer the following information for your consideration.

## Transportation Summary

The petition is located adjacent to Galloway Road, a privately maintained local road and Mallard Glen Drive, a City maintained local road. A Traffic Impact Study (TIS) is needed for this site. The petitioner submitted the Revised TIS on 11/12/22 and was approved on 4/17/2023. CDOT has coordinated with the petitioner to upgrade the existing John Adams Road Stub to John Adams to CLDSM U-07 and provide a public

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roadway connection street stub towards Sir Anthony Drive. All outstanding CDOT issues have been addressed.

## Trip Generation

| Scenario                        | Land Use   | Intensity  | Trip Generation (vehicle trips/day) | Source                         |
|---------------------------------|--|--|-------------------------------------|--------------------------------|
| Existing Use                    | Vacant   | -  | -                                   | Tax Record                     |
| Entitlement with Current Zoning | Single Family Detached (R-3, 182.71 acres)   | 548 Dwelling Units                                       | 4,830                               | General Guidance from Planning |
| Proposed Zoning                 | Multifamily (Low-Rise)<br>Single Family Attached<br>Elementary School (MX-2, 182.71 acres) | 975 Dwelling Units<br>975 Dwelling Units<br>900 Students | 14,815                              | Per TIS Scope                  |

Provide comments to the specified comments below.

## Outstanding Issues

**Strikethrough = Resolved**

### 1. Traffic Study:

**~~Outstanding Comment from 08-25-22:~~**

~~A Traffic Impact Study (TIS) is necessary for the complete review of this petition due to the site generating more than 2,500 daily trips. The petitioner submitted the Draft TIS Scoping Package on August 16, 2022, and CDOT approved the scoping package on August 31, 2022. **The Revised TIS was submitted to CDOT on January 13, 2023, and is currently under review by CDOT.**~~

~~Staff will receive the petition and begin review, but the public hearing will not be scheduled until the TIS is approved to allow for the minimum time necessary for CDOT to review the study and reach agreement with the petitioner on the required transportation commitments. *Any agreed upon commitments shall be included on the last site plan submitted no fewer than 4 weeks prior to the public hearing.* Therefore, additional traffic mitigation comments may be forthcoming once the TIS is approved by CDOT (and NCDOT).~~

~~**Updated Comment 04-06-23:** Revise site plan conditional notes to include final improvements identified in the TIS.~~

~~**Updated Comment 04-18-23:** Revise site plan conditional notes to include the following improvements from the approved TIS:~~

- ~~• Include updated improvements for the I-85 interchange and Mallard Creek Church Road from the approved TIS that involved restriping bridge and providing dual left turn lanes at both ramps on Mallard Creek Church Road.~~
- ~~• Include updated improvements for the US-29 (Tryon Street) and US-29 Service Road from the approved TIS that involved pursuing a full movement traffic signal and turn lane improvements.~~
- ~~• There will be no required improvements at the Mallard Creek Church Road and John Adams Road intersection now.~~

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~~Updated Comment 04-25-23: Revise site plan conditional notes to include the following improvements from the approved TIS:~~

- ~~• The Mallard Creek Church Road and Mallard Glen Drive intersection improvements need to have two separate improvements with regards to the southbound left turn lane as shown below:
  - ~~○ "Provide a southbound left turn lane on Mallard Glen Drive with 150 feet of storage and appropriate deceleration/taper before the first CO." This improvement would be tied prior to the first CO.~~
  - ~~○ "Provide a second southbound left turn lane on Mallard Glen Drive with 150 feet of storage and appropriate deceleration/taper prior to 404<sup>st</sup> residential CO." The improvement would be tied prior to the 404<sup>st</sup> CO.~~~~
- ~~• The Mallard Glen Drive and Mallard Highlands Drive intersection improvement needs to be updated to being tied prior to 404<sup>st</sup> CO. Second receiving lane on Mallard Glen Drive is not needed initially.~~

~~Clarifying Comment 05-24-23: Revise site plan conditional notes to include the following improvements from the approved TIS:~~

- ~~• The southbound Mallard Glen Drive approach currently only has a one lane approach and phase 1 will add one left turn lane. See below for the highlighted portion to be removed.~~

*REMOVE*

2. Provide a second southbound left turn lane on Mallard Glen Drive with 150 feet of storage and appropriate deceleration / taper prior to 1<sup>st</sup> residential certificate of occupancy;

- ~~• Check the two highlighted improvements below. It is the same improvement and they have different CO triggers. This improvement should be tied prior to 404<sup>st</sup> CO. Make both highlighted portions tied prior to the 404<sup>st</sup> CO.~~

### #3: Intersection of Mallard Creek Church Road at Mallard Glen Drive

1. Install traffic signal prior to 1<sup>st</sup> residential certificate of occupancy;
2. Provide a second southbound left turn lane on Mallard Glen Drive with 150 feet of storage and appropriate deceleration / taper prior to 1<sup>st</sup> residential certificate of occupancy;
3. On southbound Mallard Glen Drive, provide a total of two (2) left turn lanes and two (2) right turn lanes prior to 404<sup>st</sup> residential certificate of occupancy. The leftmost left turn lane is to have 150 feet of storage and appropriate deceleration taper, and the other left turn lane is to be full-length. Both right turn lanes are to have 200 feet of storage and appropriate deceleration / taper;
4. Provide a second eastbound left turn lane on Mallard Creek Church Road with 300 feet of storage and appropriate deceleration / taper prior to 404<sup>st</sup> residential certificate of occupancy; and
5. Provide a second receiving lane on inbound Mallard Glen Drive with appropriate length of full lane width and merge taper prior to 404<sup>st</sup> residential certificate of occupancy. Based on available right of way, it is anticipated that this lane can be up to 900 feet in length, as measured from Mallard Creek Church Road.

### #9: Mallard Glen Drive at Mallard Highlands Drive

1. Provide additional northbound receiving lane on Mallard Glen Drive with appropriate length of full lane width and merge taper, prior to 1<sup>st</sup> residential certificate of occupancy. Based on available right of way, it is anticipated that this lane can be up to 900 feet in length, as measured from Mallard Creek Church Road.

~~2. New CDOT COMMENT 04-06-23: Revise site plan and conditional notes by committing to upgrading Northbend Drive and Heritage Lake Drive to public street standards.~~

~~Clarifying Comment 04-25-23: Revise conditional note to say "Petitioner shall coordinate with CDOT to upgrade pavement and stormwater infrastructure along Northbend Drive and Heritage Lake Drive to public street standards in coordination with CDOT street acceptance team during permitting, as improvements are reasonably able to be completed within the existing right of way and no additional right of way acquisition would be required to perform the improvements"~~

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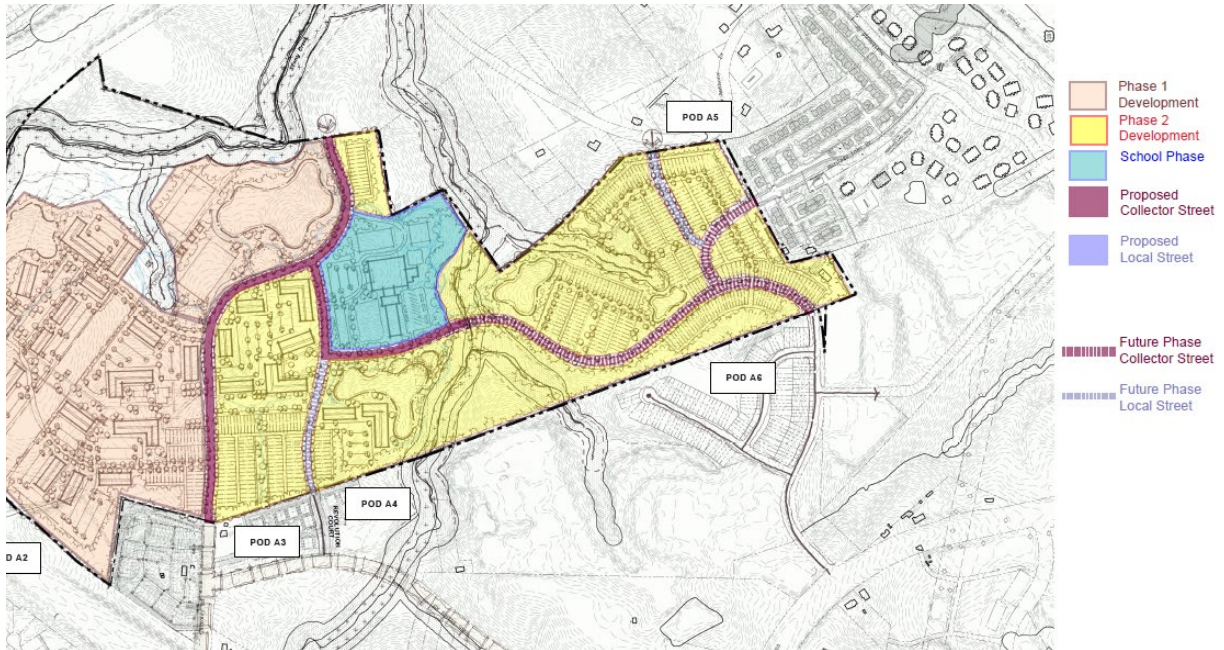
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- ~~3. Outstanding Comment from 08-25-22: Revise site plan and conditional note(s) to commit to improving the existing Galloway Road stub to John Adams to the local collector street typical section per U-07.~~



- ~~4. Revise site plan and conditional note(s) to commit to the proposed roadway network typical sections and proposed phasing outlined in the Traffic Impact Study. Provide typical sections per CLDSM for all proposed public and private streets. Provide 8-foot sidewalk and 8-foot planting strip on all public streets.~~

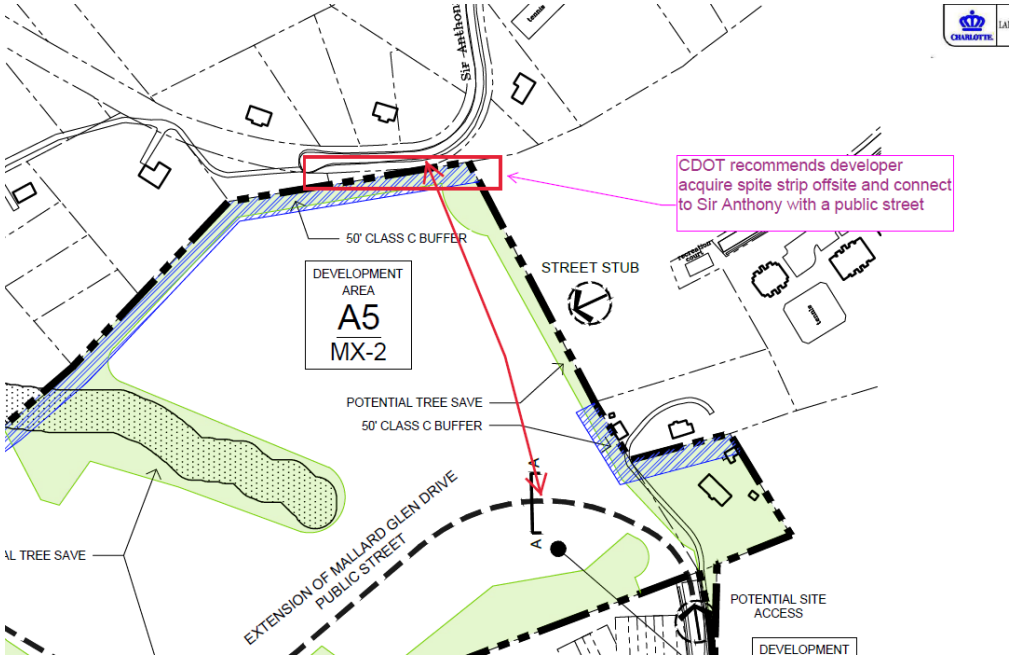


- ~~5. Add conditional Lighting Note for petitioner to coordinate with CDOT Lighting, Bruce Horltdt (704-589-5836 or Bruce.Horltdt@charlottenc.gov), prior to permitting of lighting fixtures on public roadways.~~
- ~~6. **CDOT RECOMMENDATION:** CDOT recommends the petitioner commit to providing a public street connection to Sir Anthony Drive.~~

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## Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
7. The petitioner needs to complete and submit the [Right of way Abandonment Petition form](#) to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.
8. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:  
<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>