

URBAN DESIGN PARTNERS 1318-e6 central ave. P 704.334.3303 charlotte, nc 28205 F 704.334.3305 urbandesignpartners.com nc firm no: P-0418 sc coa no: C-03044 NVR, Inc. munity Com Sheet Technical Data Wallace Lane, Charlotte, NC Wallace REVISIONS: PER PLANNING STAFF COMMEN PER PLANNING STAFF COMMEN PER PLANNING STAFF COMMEN PER PLANNING STAFF COMMEN BY: UDP UDP UDP DATE: 03.15.21 04.12.21 05.11.21 06.24.21 ON - α 6 4 Project No: 20-CLT-042 Date: 11.12.2020 Designed By: UDP Checked By: GPP Sheet No: GRAPHIC SCALE RZ-1.0 1 INCH = 50 FEET

FUTURE B.O.C. LOCATED 17.5' FRI EXIST. WALLACE LI CENTERLINE

(FROM FUTURE B.O.C.) - HATCH IDENTIFIES FUTURE R/W AREA TO BE DEDICATED

N/F LAVERN M SHARPE, TAMMRA G SHARPE DB 26478-790 ZONED: R-4

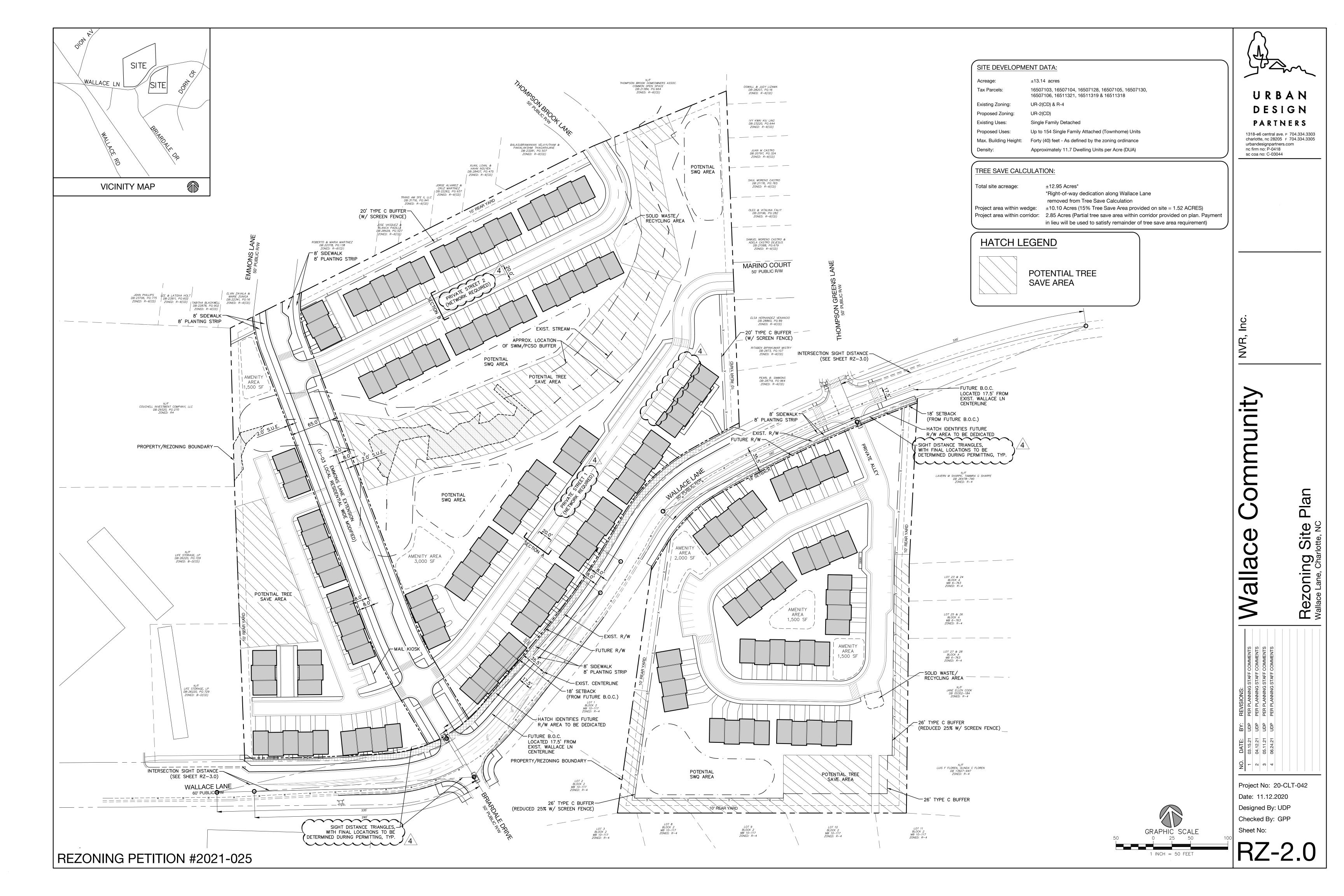
LOT 23 & 24 BLOCK A MB 6–763 ZONED: R–4

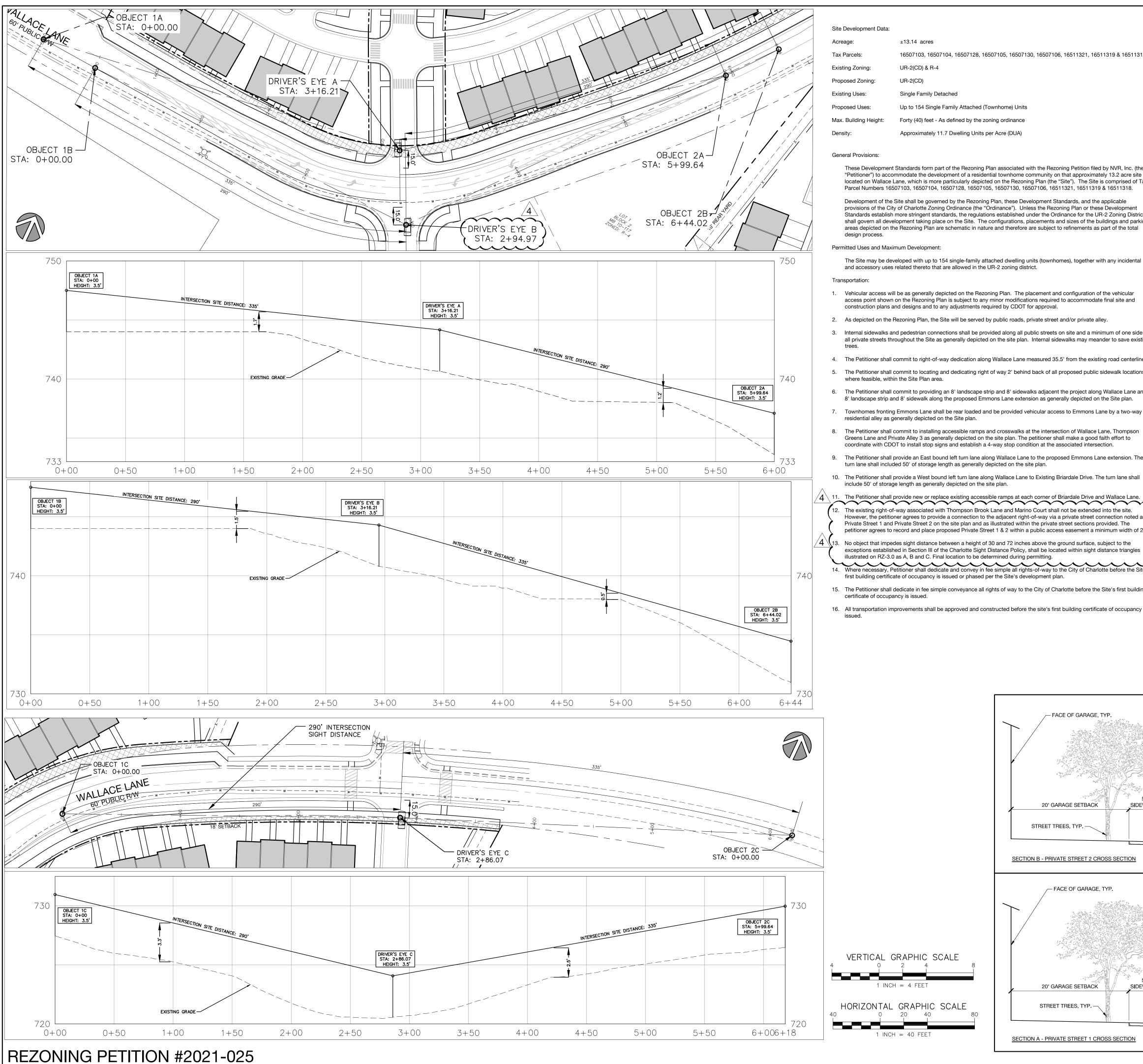
LOT 25 & 26 BLOCK A MB 6–763 ZONED: R–4

LOT 27 & 28 BLOCK A MB 6-763 ZONED: R-4

N/F JANE ELLEN COOK DB 05302–184 ZONED: R–4

N/F LUIS F FLOREN, OLINDA C FLOREN DB 13927-697 ZONED: R-4





creage:	±13.14 acres
ax Parcels:	16507103, 16507104, 16507128, 16507105, 16507130, 16507106, 16511321, 165113
isting Zoning:	UR-2(CD) & R-4
oposed Zoning:	UR-2(CD)
isting Uses:	Single Family Detached
oposed Uses:	Up to 154 Single Family Attached (Townhome) Units
ax. Building Height:	Forty (40) feet - As defined by the zoning ordinance
ensity:	Approximately 11.7 Dwelling Units per Acre (DUA)

These Development Standards form part of the Rezoning Plan associated with the Rezoning Petition filed by NVR, Inc. (the "Petitioner") to accommodate the development of a residential townhome community on that approximately 13.2 acre site located on Wallace Lane, which is more particularly depicted on the Rezoning Plan (the "Site"). The Site is comprised of Tax Parcel Numbers 16507103, 16507104, 16507128, 16507105, 16507130, 16507106, 16511321, 16511319 & 16511318. Development of the Site shall be governed by the Rezoning Plan, these Development Standards, and the applicable provisions of the City of Charlotte Zoning Ordinance (the "Ordinance"). Unless the Rezoning Plan or these Development Standards establish more stringent standards, the regulations established under the Ordinance for the UR-2 Zoning District shall govern all development taking place on the Site. The configurations, placements and sizes of the buildings and parking

Permitted Uses and Maximum Development:

The Site may be developed with up to 154 single-family attached dwelling units (townhomes), together with any incidental and accessory uses related thereto that are allowed in the UR-2 zoning district.

- Vehicular access will be as generally depicted on the Rezoning Plan. The placement and configuration of the vehicular access point shown on the Rezoning Plan is subject to any minor modifications required to accommodate final site and construction plans and designs and to any adjustments required by CDOT for approval.
- 2. As depicted on the Rezoning Plan, the Site will be served by public roads, private street and/or private alley.
- Internal sidewalks and pedestrian connections shall be provided along all public streets on site and a minimum of one side of all private streets throughout the Site as generally depicted on the site plan. Internal sidewalks may meander to save existing
- 4. The Petitioner shall commit to right-of-way dedication along Wallace Lane measured 35.5' from the existing road centerline. 5. The Petitioner shall commit to locating and dedicating right of way 2' behind back of all proposed public sidewalk locations,
- where feasible, within the Site Plan area. 6. The Petitioner shall commit to providing an 8' landscape strip and 8' sidewalks adjacent the project along Wallace Lane and
- 8' landscape strip and 8' sidewalk along the proposed Emmons Lane extension as generally depicted on the Site plan. 7. Townhomes fronting Emmons Lane shall be rear loaded and be provided vehicular access to Emmons Lane by a two-way
- The Petitioner shall commit to installing accessible ramps and crosswalks at the intersection of Wallace Lane, Thompson Greens Lane and Private Alley 3 as generally depicted on the site plan. The petitioner shall make a good faith effort to coordinate with CDOT to install stop signs and establish a 4-way stop condition at the associated intersection.
- 9. The Petitioner shall provide an East bound left turn lane along Wallace Lane to the proposed Emmons Lane extension. The turn lane shall included 50' of storage length as generally depicted on the site plan.
- 10. The Petitioner shall provide a West bound left turn lane along Wallace Lane to Existing Briardale Drive. The turn lane shall include 50' of storage length as generally depicted on the site plan.
- 1. The Petitioner shall provide new or replace existing accessible ramps at each corner of Briardale Drive and Wallace Lane. 2. The existing right-of-way associated with Thompson Brook Lane and Marino Court shall not be extended into the site. However, the petitioner agrees to provide a connection to the adjacent right-of-way via a private street connection noted as Private Street 1 and Private Street 2 on the site plan and as illustrated within the private street sections provided. The petitioner agrees to record and place proposed Private Street 1 & 2 within a public access easement a minimum width of 29'.
- No object that impedes sight distance between a height of 30 and 72 inches above the ground surface, subject to the exceptions established in Section III of the Charlotte Sight Distance Policy, shall be located within sight distance triangles illustrated on RZ-3.0 as A, B and C. Final location to be determined during permitting.
- 14. Where necessary, Petitioner shall dedicate and convey in fee simple all rights-of-way to the City of Charlotte before the Site's first building certificate of occupancy is issued or phased per the Site's development plan.
- 15. The Petitioner shall dedicate in fee simple conveyance all rights of way to the City of Charlotte before the Site's first building certificate of occupancy is issued.
- 16. All transportation improvements shall be approved and constructed before the site's first building certificate of occupancy is

