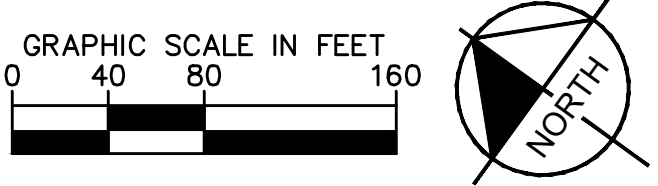


ROADWAY, PEDESTRIAN, AND CYCLE TRACK IMPROVEMENTS ASSOCIATED WITH BAXTER STREET EXTENSION SECTION E-E SHALL CONNECT TO THE RELATIVE INFRASTRUCTURE ASSOCIATED WITH THE PEARL PARKWAY EXTENSION, SDRMUR-2020-00026

LEGEND	
	PROPOSED REZONING BOUNDARY
	EXISTING CITY MAINTAINED ROAD
	PROPOSED CITY MAINTAINED ROAD
	EXISTING PROPERTY LINE
	EXISTING ROW LINE
	DEVELOPMENT AREA A
	DEVELOPMENT AREA B
	DEVELOPMENT AREA C
	DEVELOPMENT AREA D

SITE DEVELOPMENT DATA:	
REZONING BOUNDARY AREA:	± 14.28 ACRES
TAX PARCEL #S:	125-201-50, 125-201-54, 125-201-58, 125-201-05, 125-201-07, 125-201-10, 125-201-11, 125-201-55, 125-201-63
EXISTING ZONING:	O-2 (PED), MUDD, MUDD (CD), B-1 (PED), B-2 (PED)
PROPOSED ZONING:	MUDD-O (PED) WITH 5-YEAR VESTED RIGHTS
PROPOSED USES:	PER DEVELOPMENT STANDARDS
PROPOSED MAXIMUM GROSS FLOOR AREA:	PER DEVELOPMENT STANDARDS
MAXIMUM BUILDING HEIGHT:	PER DEVELOPABLE BUILDING HEIGHTS RZ-1.3
PARKING:	PER ORDINANCE REQUIREMENTS



NO.	REVISIONS	DATE	BY

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 NC LICENSE #F-0102

KH PROJECT	015031128
DATE	08/16/2021
SCALE	AS SHOWN
DESIGNED BY	AW
DRAWN BY	AW
CHECKED BY	MP

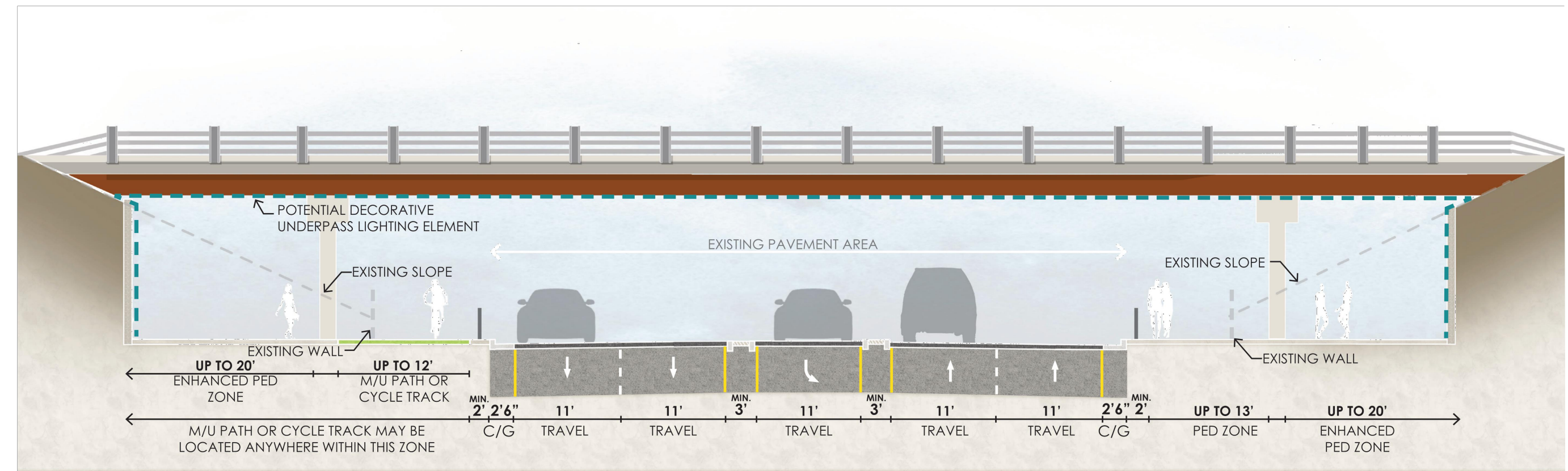
TECHNICAL DATA
SHEET

CHARLOTTE
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 NORTH CAROLINA
 REZONING PETITION No.2021-092

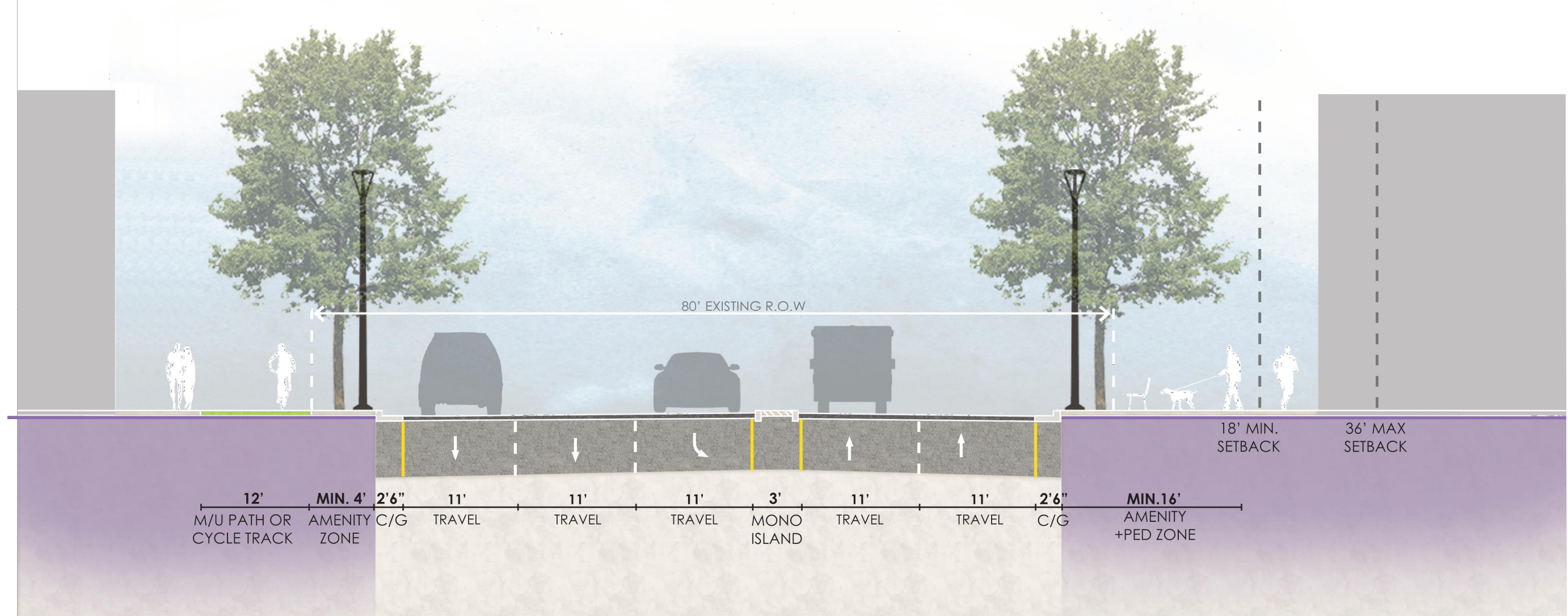
SHEET NUMBER
RZ-1.0

Plotted By: Camille Post - Sheet: Snt: Atrium - CKC Rezoning - Layout: RZ-1.0 - TECHNICAL DATA SHEET - November 18, 2021 - 04:40:35pm - \\kimley-horn.com\SE_CHELL\CHL_PRA\015031128_CKC Rezoning and Design\02 - DWG\Plan\sheet\A\1.0 - Rezoning_RZ-1.0 - TECHNICAL DATA SHEET

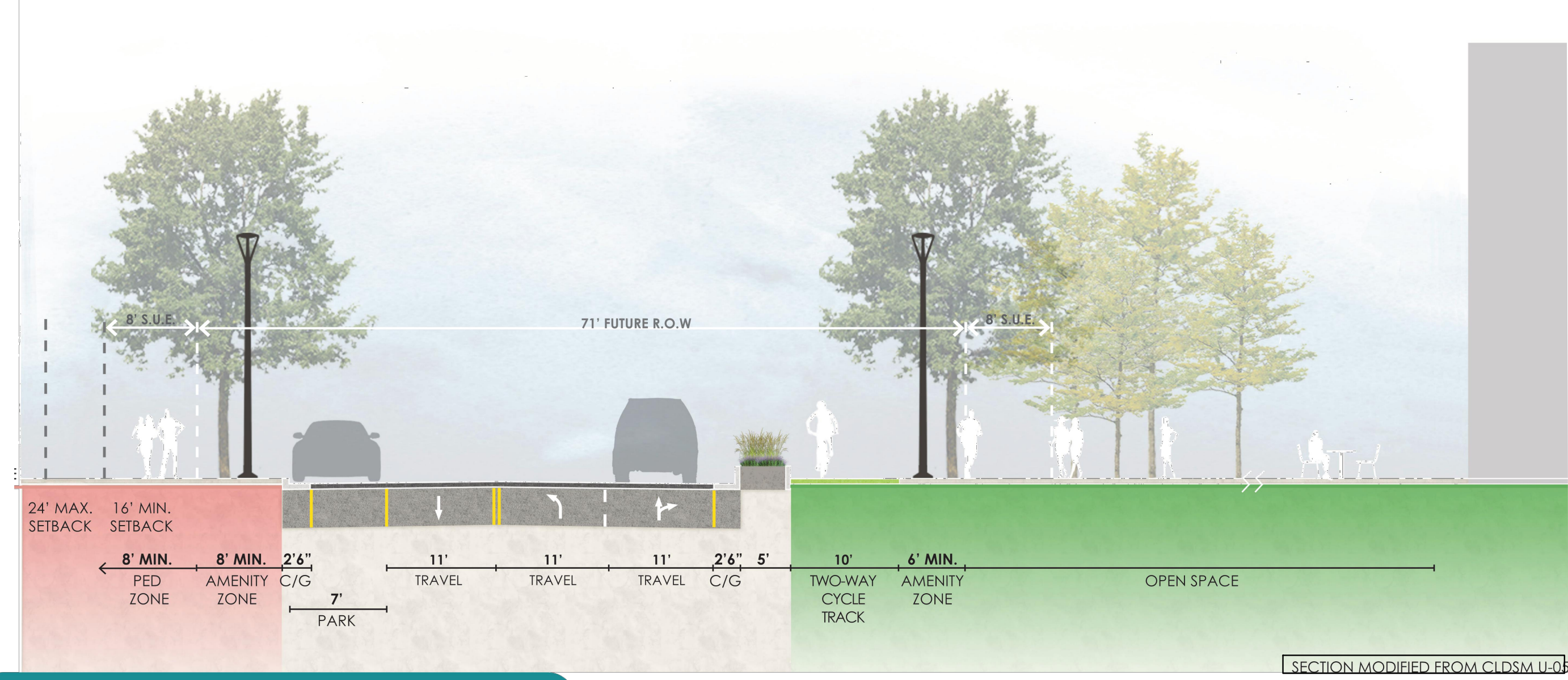
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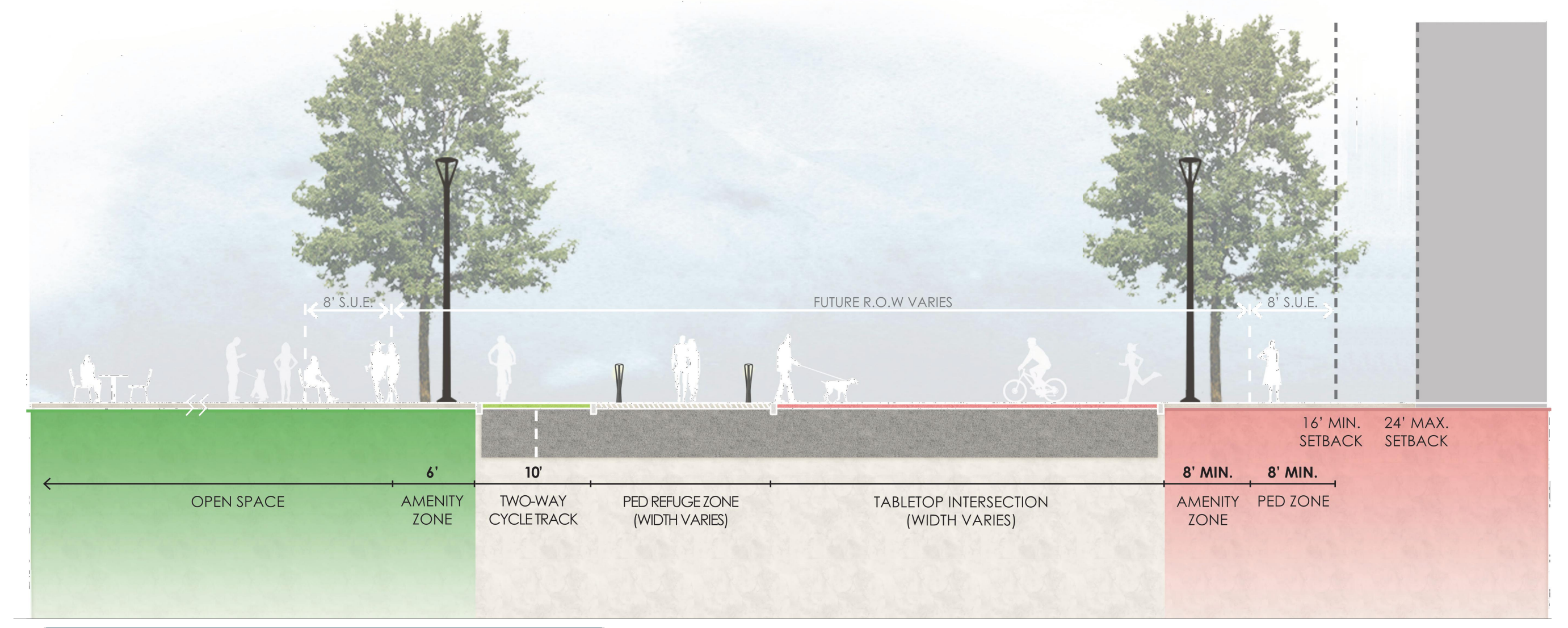
SECTION A-A S. MCDOWELL AT I-277 UNDERPASS



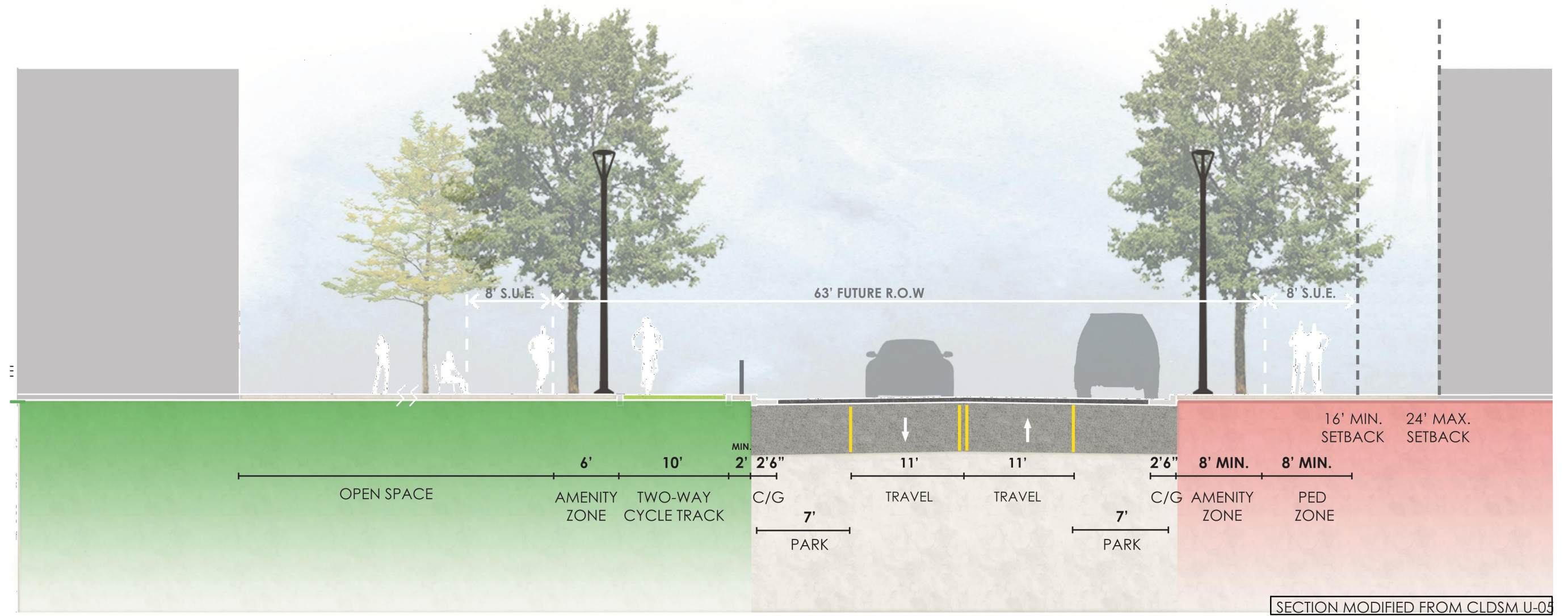
SECTION B-B S. MCDOWELL AT BAXTER STREET



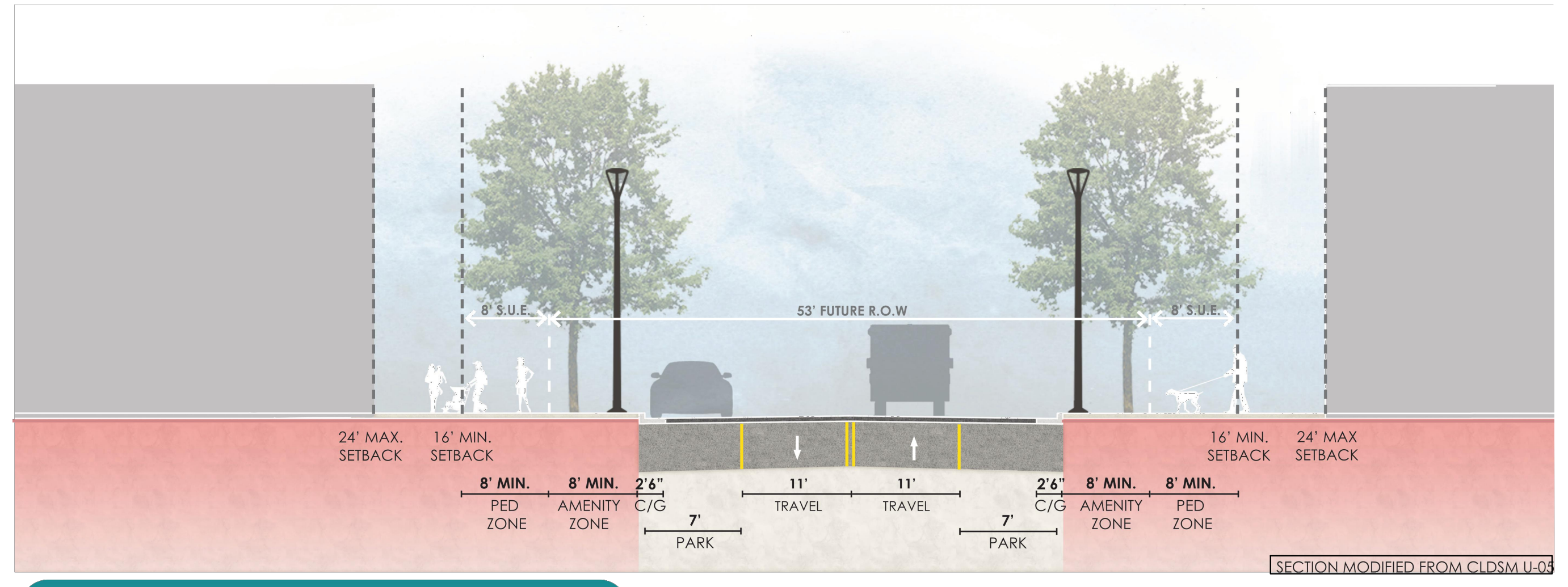
SECTION C-C BAXTER STREET



SECTION D-D BAXTER STREET



SECTION E-E PEARL PARKWAY



SECTION F-F NEW ROADWAY

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KH PROJECT	015031128
DATE	08/16/2021
SCALE	AS SHOWN
DESIGNED BY	BN
DRAWN BY	BN
CHECKED BY	KE

TYPICAL SECTIONS
 REZONING PETITION No.2021-092

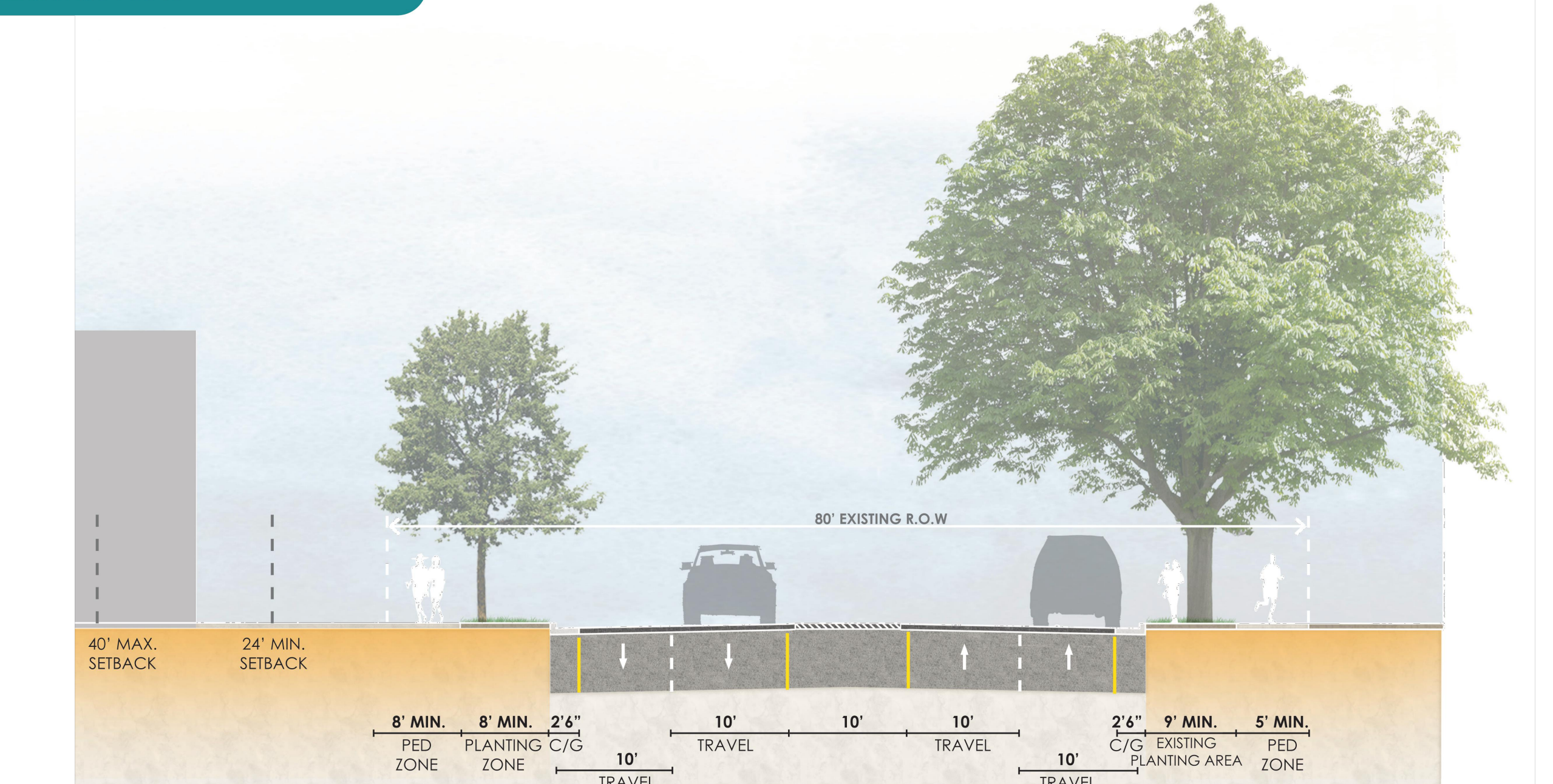
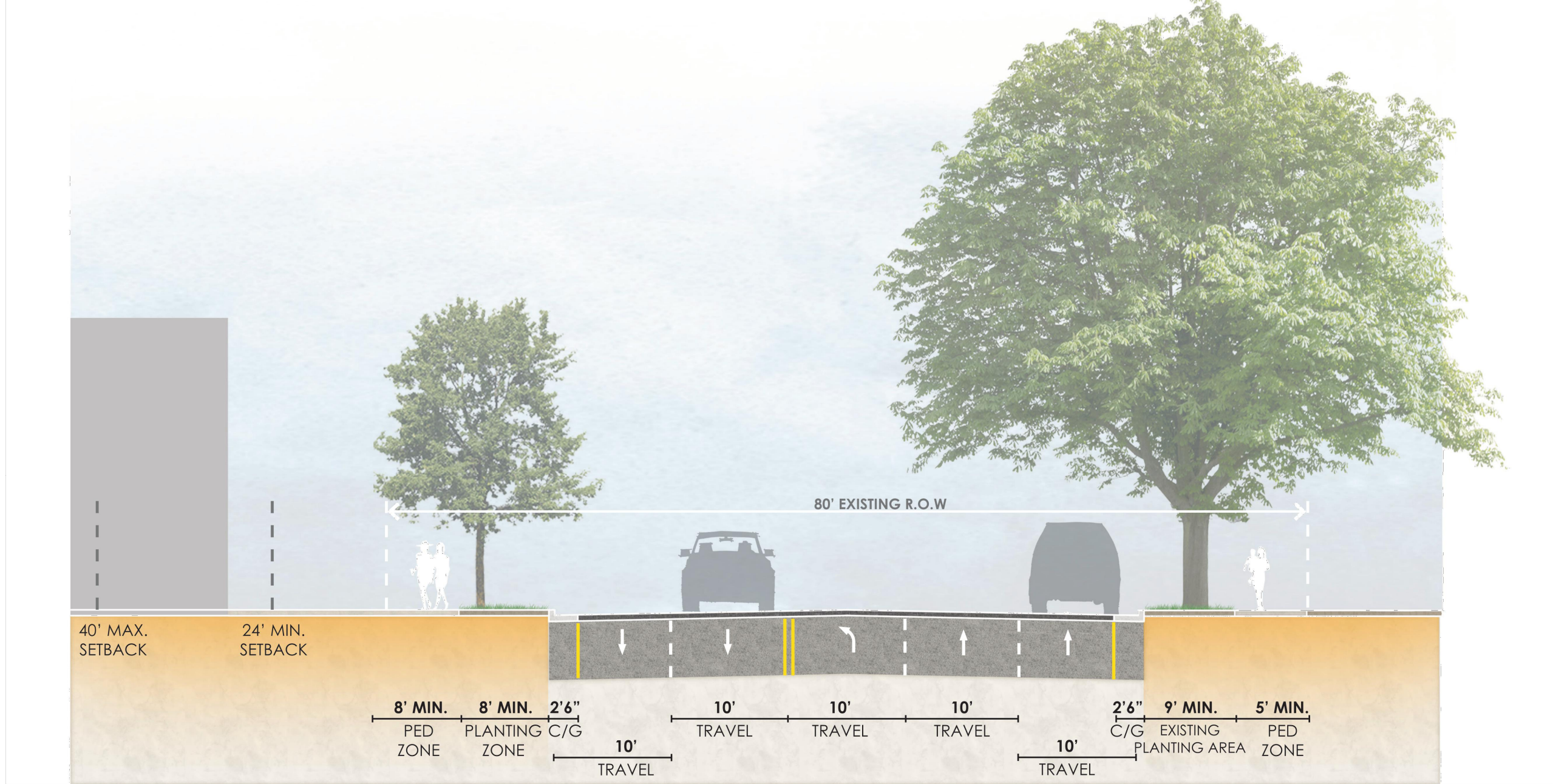
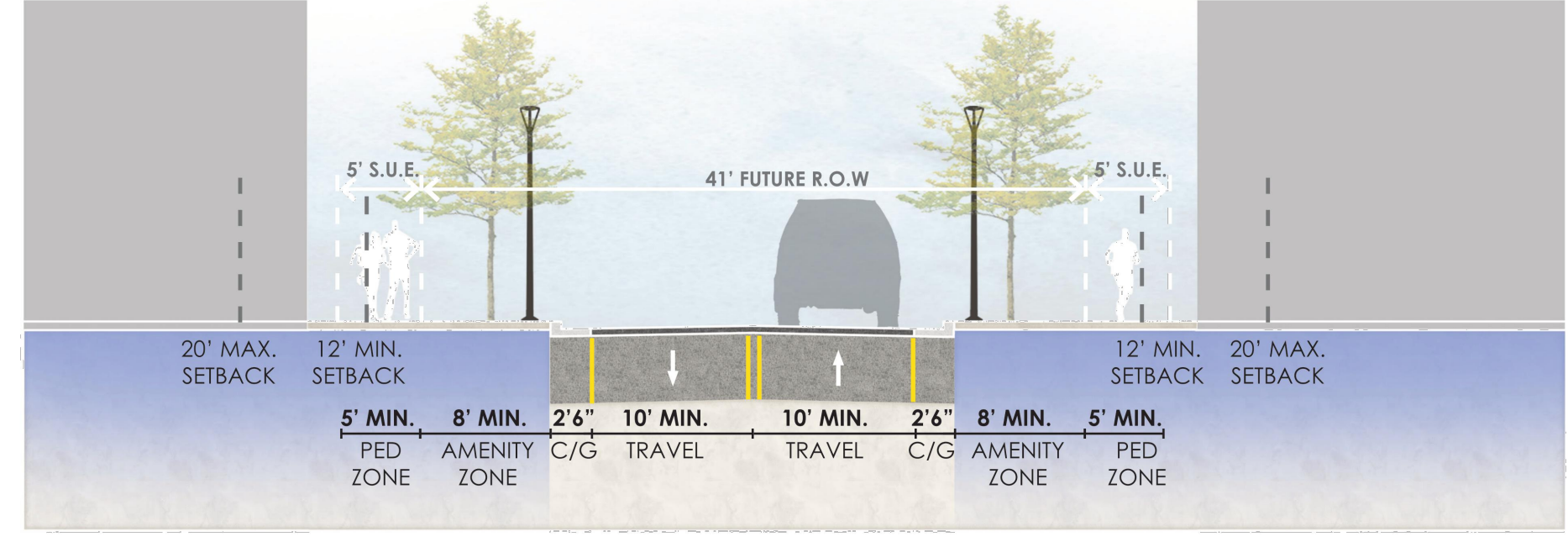
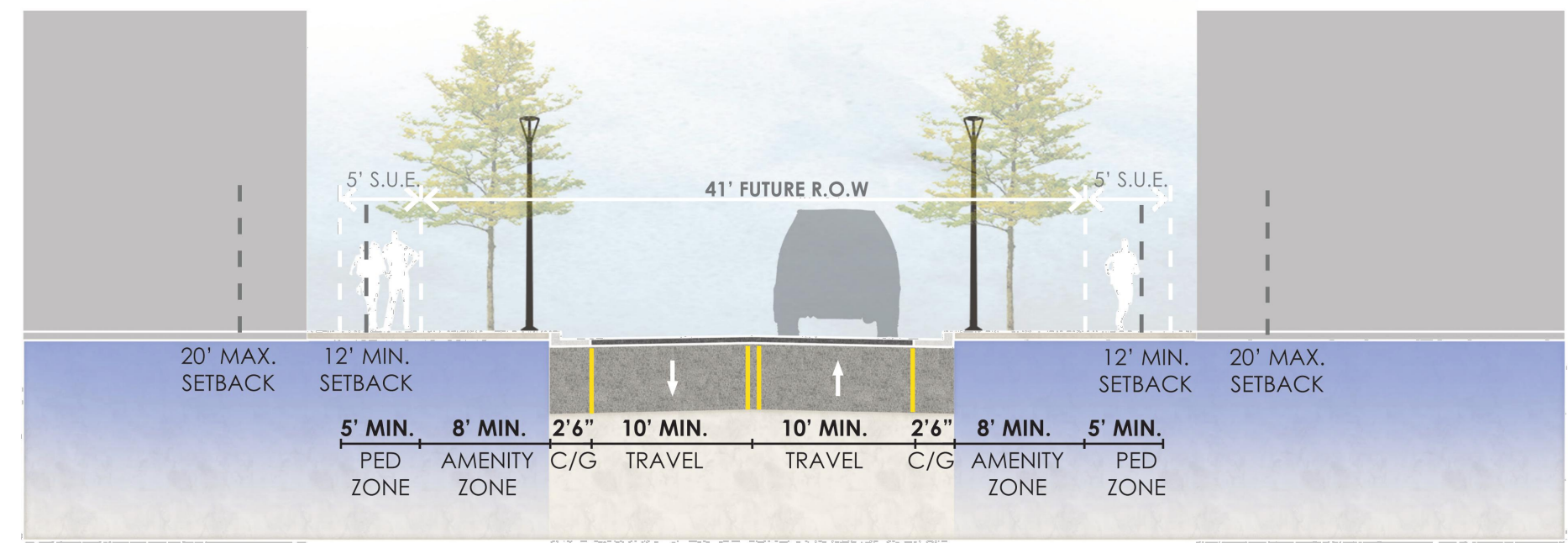
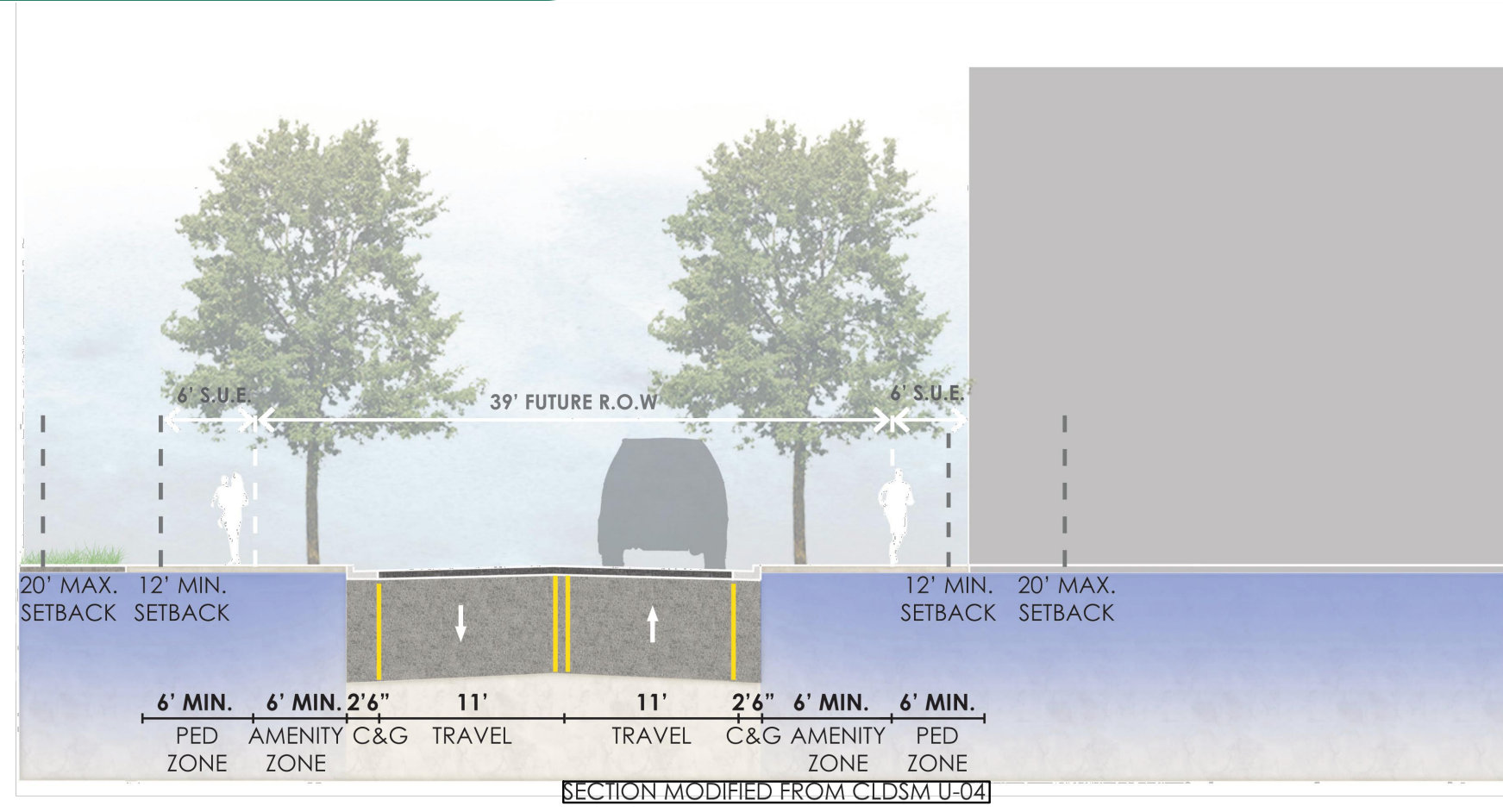
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 NORTH CAROLINA
 CITY OF CHARLOTTE

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NOTES:
 1. SUE LOCATIONS TO BE DETERMINED DURING SITE PERMITTING
 2. ALL LANE DIMENSIONS SHOWN INDICATE THE MINIMUM REQUIRED LANE WIDTH

APPROVED BY CITY COUNCIL
 RZP-2021-092
 1/24/2022



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DATE	08/16/2021
SCALE	AS SHOWN
DESIGNED BY	BN
DRAWN BY	BN
CHECKED BY	KE

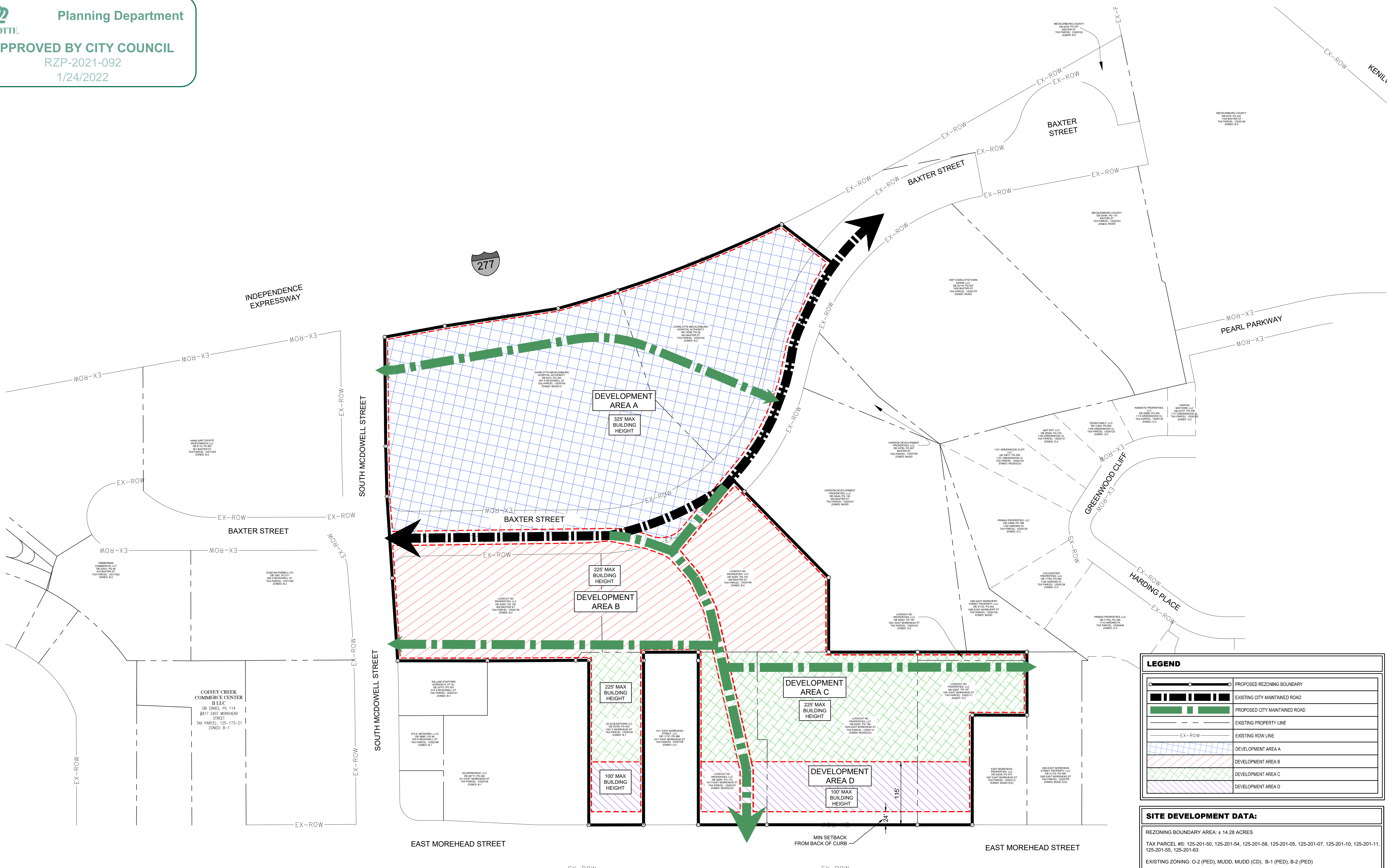
TYPICAL SECTIONS
 REZONING PETITION No.2021-092

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 CITY OF CHARLOTTE

SHEET NUMBER
RZ-1.2

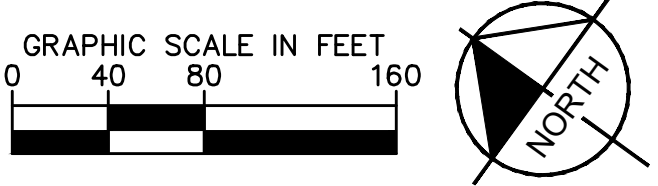
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Plotted By: Cahill, Pat. Sheet: Set: Atrium - CRK Rezoning. Layout: RZ-1.3 DEVELOPABLE BUILDING HEIGHTS. November 18, 2021. 04:41:19pm. \\kimley-horn.com\SE\CHL\CHL_PRA\015031 Carolina Healthcare System\25 CRK Rezoning and Design\02 - DWG\PlanSheets\1-0 - Rezoning_RZ\1.2 BLDG HEIGHTS



LEGEND	
	PROPOSED REZONING BOUNDARY
	EXISTING CITY MAINTAINED ROAD
	PROPOSED CITY MAINTAINED ROAD
	EXISTING PROPERTY LINE
	EXISTING ROW LINE
	DEVELOPMENT AREA A
	DEVELOPMENT AREA B
	DEVELOPMENT AREA C
	DEVELOPMENT AREA D

SITE DEVELOPMENT DATA:	
REZONING BOUNDARY AREA:	± 14.28 ACRES
TAX PARCEL #S:	125-201-50, 125-201-54, 125-201-55, 125-201-07, 125-201-10, 125-201-11, 125-201-55, 125-201-63
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PROPOSED MAXIMUM GROSS FLOOR AREA:	PER DEVELOPMENT STANDARDS
MAXIMUM BUILDING HEIGHT:	PER DEVELOPABLE BUILDING HEIGHTS RZ-1.3
PARKING:	PER ORDINANCE REQUIREMENTS



No.	REVISIONS	DATE	BY

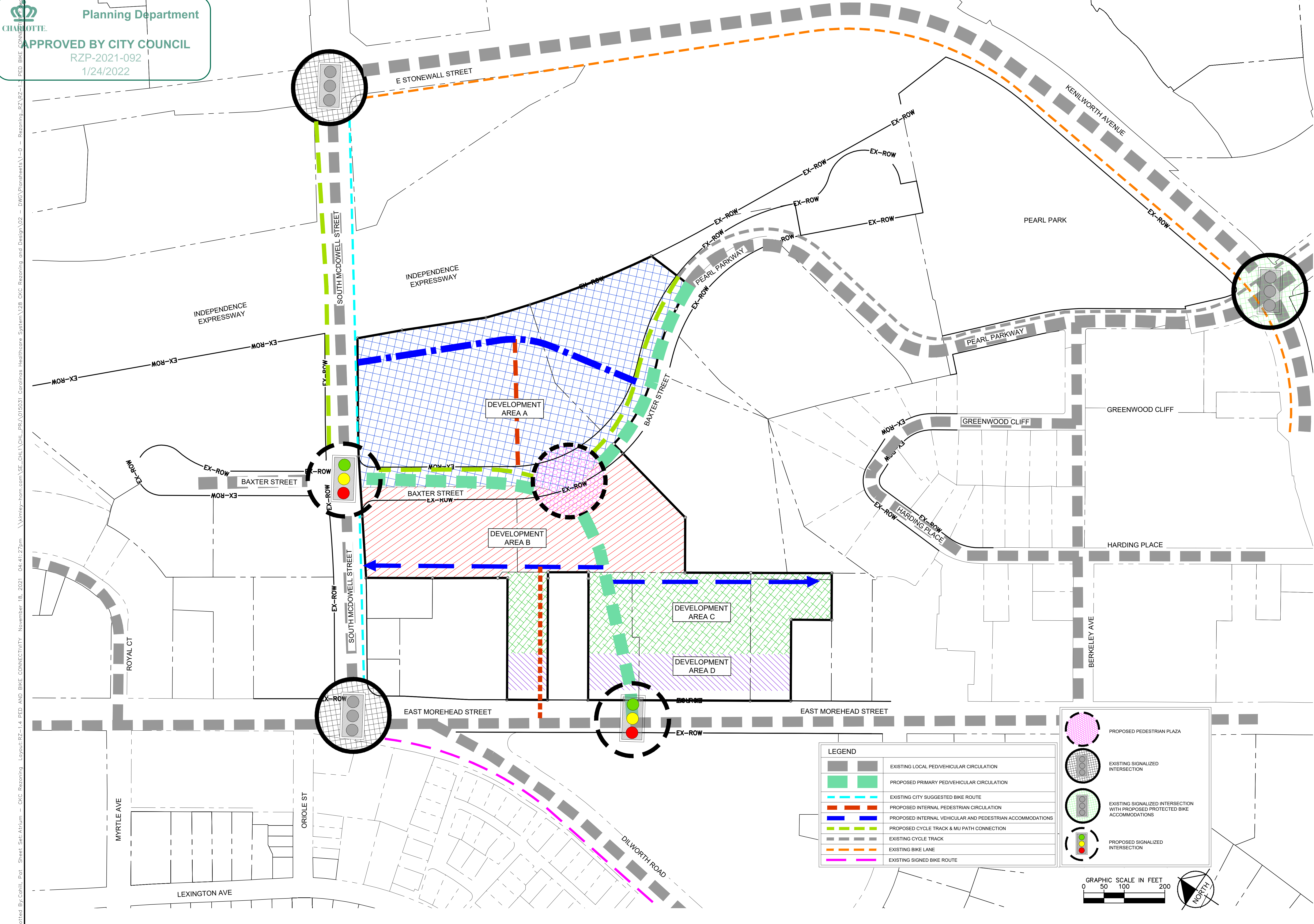
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KH PROJECT	015031128
DATE	08/16/2021
SCALE	AS SHOWN
DESIGNED BY	AW
DRAWN BY	AW
CHECKED BY	MP

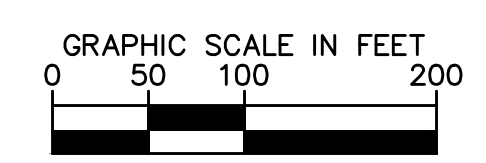
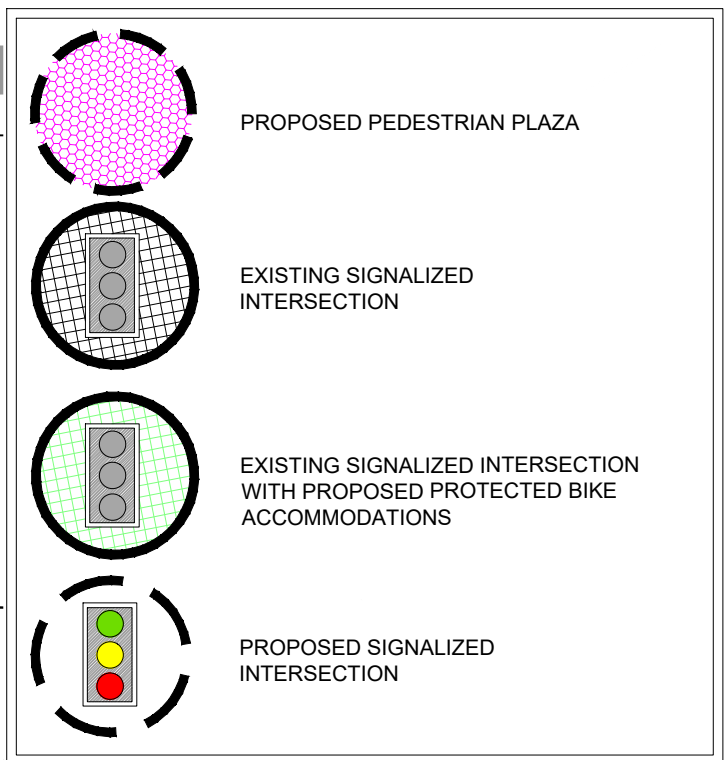
DEVELOPABLE BUILDING HEIGHTS

CHARLOTTE INNOVATION COMMUNITY PREPARED FOR ATRIUM HEALTH

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LEGEND	
	EXISTING LOCAL PED/VEHICULAR CIRCULATION
	PROPOSED PRIMARY PED/VEHICULAR CIRCULATION
	EXISTING CITY SUGGESTED BIKE ROUTE
	PROPOSED INTERNAL PEDESTRIAN CIRCULATION
	PROPOSED INTERNAL VEHICULAR AND PEDESTRIAN ACCOMMODATIONS
	PROPOSED CYCLE TRACK & MU PATH CONNECTION
	EXISTING CYCLE TRACK
	EXISTING BIKE LANE
	EXISTING SIGNED BIKE ROUTE



NO.	REVISIONS	BY	DATE

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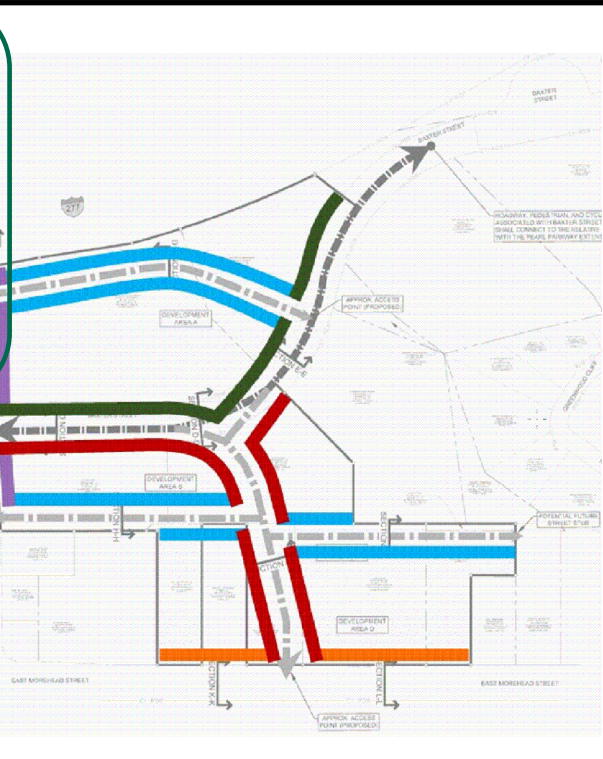
KH PROJECT	015031128
DATE	08/16/2021
SCALE	AS SHOWN
DESIGNED BY	BN
DRAWN BY	BN
CHECKED BY	KB

PED AND BIKE CONNECTIVITY
 REZONING PETITION No.2021-092

CHARLOTTE
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ATRIUM HEALTH
 NORTH CAROLINA
 SHEET NUMBER
RZ-1.4

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Charlotte Planning Department APPROVED BY CITY COUNCIL RZ-2.1 McDowell092 Morehead



(2) BUILDING DIMENSIONS (BUILDING/WALL LENGTH)
(a) IN INSTANCES WHERE THE URBAN WALL IS 450 FEET OR LONGER, AN INTERRUPTION OR BREAK IN THE URBAN WALL IS REQUIRED. PEDESTRIAN WAYS MAY ACCOMPANY THESE INTERRUPTIONS OR BREAKS TO PROVIDE ACCESS THROUGH A BLOCK OR ACCESS TO OPEN SPACES OR PARKING AT THE INTERIOR OF THE BLOCK...

(3) BUILDING SITING
(a) SETBACKS SHALL BE AS SET OUT BELOW BASED ON STREET FRONTAGE TYPE.
(i) MCDOWELL STREET: A MINIMUM 18 FOOT SETBACK AND A MAXIMUM 36 FOOT SETBACK AS MEASURED FROM BACK OF CURB.

(4) MINIMUM BUILD-TO PERCENTAGE BY STREET FRONTAGE
(a) FOR PURPOSES OF THIS SECTION 6.C(4) OF THE DEVELOPMENT STANDARDS, "BUILD-TO ZONE" SHALL MEAN THE AREA BETWEEN THE MINIMUM SETBACK AND THE MAXIMUM SETBACK, WHICH MINIMUM AND MAXIMUM SETBACKS ARE SET OUT ABOVE IN SECTION 6.C(3).

(5) REQUIRED HEIGHT STEPBACK ON DEVELOPMENT AREA D
(a) FOR ANY BUILDING LOCATED ON DEVELOPMENT AREA D, BUILDING HEIGHT ABOVE 80 FEET SHALL REQUIRE A MINIMUM BUILDING STEPBACK OF 10 FEET FROM THE BUILDING FAÇADE LINE FACING EAST MOREHEAD STREET. A CORNER TOWER ELEMENT (CORNER LOTS ONLY) LIMITED IN WIDTH AND DEPTH TO A MAXIMUM OF 25% OF THE BUILDING FRONTAGE MAY EXCEED THE HEIGHT AT WHICH A STEPBACK IS REQUIRED, BUT MAY NOT EXCEED THE ALLOWED MAXIMUM BUILDING HEIGHT.

(7) NON-RESIDENTIAL, MIXED-USE AND MULTI-FAMILY STACKED DESIGN STANDARDS
(a) TABLE 6(7) BELOW CONTAINS THE DESIGN STANDARDS FOR NON-RESIDENTIAL, MIXED USE AND MULTI-FAMILY STACKED BUILDINGS ON THE SITE. TABLE 6(7) ASSIGNS DESIGN STANDARDS TO BUILDING FAÇADES ALONG THE DIFFERENT TYPES OF STREET FRONTAGES ON THE SITE. AN "X" INDICATES THAT THE SPECIFIC DESIGN STANDARD APPLIES.

Table 6(7) with columns: FRONTAGE TYPE, MCDOWELL, MOREHEAD, DISTRICT MAIN STREET, OPEN SPACE, TERTIARY. Rows describe various facade and entrance requirements.

PROMINENT ENTRANCE DEFINITION: A BUILDING ENTRANCE THAT IS VISUALLY DISTINCTIVE FROM THE REMAINING PORTIONS OF THE FAÇADE WHERE IT IS LOCATED.
(i) FOR NONRESIDENTIAL, MIXED-USE AND MULTI-FAMILY STACKED BUILDINGS, ENTRANCES THAT CONTAIN AT LEAST THREE OF THE FOLLOWING ARE CONSIDERED A PROMINENT ENTRANCE...

- (8) PARKING GARAGE DESIGN STANDARDS.
(a) GENERAL PARKING GARAGE DESIGN REGULATIONS.
(i) ALL PARKING GARAGES SHALL BE SCREENED, WRAPPED, OR TREATED BASED ON STREET FRONTAGE TYPE AS INDICATED IN TABLE 6(8) BELOW.

Table 6(8) with columns: FRONTAGE TYPE, MCDOWELL, MOREHEAD, DISTRICT MAIN STREET, OPEN SPACE, TERTIARY. Rows describe parking garage screening requirements.

- 7. STREETScape/LANDSCAPING
A. PLANTING STRIPS AND SIDEWALKS SHALL BE INSTALLED IN ACCORDANCE WITH THE STREET CROSS SECTIONS SET OUT ON SHEETS RZ-1.1 AND RZ-1.2 OF THE REZONING PLAN.
B. DUMPSTER AND RECYCLING AREAS WILL BE ENCLOSED ON ALL FOUR SIDES BY AN OPAQUE WALL OR FENCE WITH ONE SIDE BEING A HINGED OPAQUE GATE...

- 9. OPEN SPACE
A. AS NOTED ABOVE IN PARAGRAPH 1.F, THE SITE SHALL BE CONSIDERED TO BE A PLANNED/UNIFIED DEVELOPMENT. ACCORDINGLY, THE OPEN SPACE REQUIREMENTS SHALL BE CALCULATED AND SATISFIED OVER THE ENTIRE SITE, RATHER THAN WITHIN EACH INDIVIDUAL DEVELOPMENT AREA OR WITHIN EACH INDIVIDUAL BUILDING SITE OR LOT...

- 10. BINDING EFFECT OF THE REZONING DOCUMENTS AND DEFINITIONS
A. IF THIS REZONING PETITION IS APPROVED, ALL CONDITIONS APPLICABLE TO THE USE AND DEVELOPMENT OF THE SITE IMPOSED UNDER THESE DEVELOPMENT STANDARDS AND THE REZONING PLAN WILL, UNLESS AMENDED IN THE MANNER PROVIDED UNDER THE ORDINANCE, BE BINDING UPON AND INURE TO THE BENEFIT OF PETITIONER AND THE CURRENT AND SUBSEQUENT OWNERS OF THE SITE AND THEIR RESPECTIVE SUCCESSORS IN INTEREST AND ASSIGNS.

Table with columns: No., REVISIONS, DATE. Row 1: 12/08/21

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Table with columns: KH PROJECT, DATE, SCALE, DESIGNED BY, DRAWN BY, CHECKED BY. Values: 015031128, 08/16/2021, AS SHOWN, etc.

CHARLOTTE INNOVATION COMMUNITY PREPARED FOR ATRIUM HEALTH CITY OF CHARLOTTE NORTH CAROLINA REZONING PETITION No.2021-092 DEVELOPMENT STANDARDS

SHEET NUMBER RZ-2.1