RZP-2021-060

2605 Central AV, Charlotte NC 28205

12/10/2021 9:04:32 AM

General Notes

[Discipline: CHARLOTTE WATER]

Note

Summary: Water and sewer service is accessible for this rezoning boundary.

Charlotte Water has accessible water system infrastructure for the rezoning boundary via an existing 6-inch water distribution main located along Central Ave.

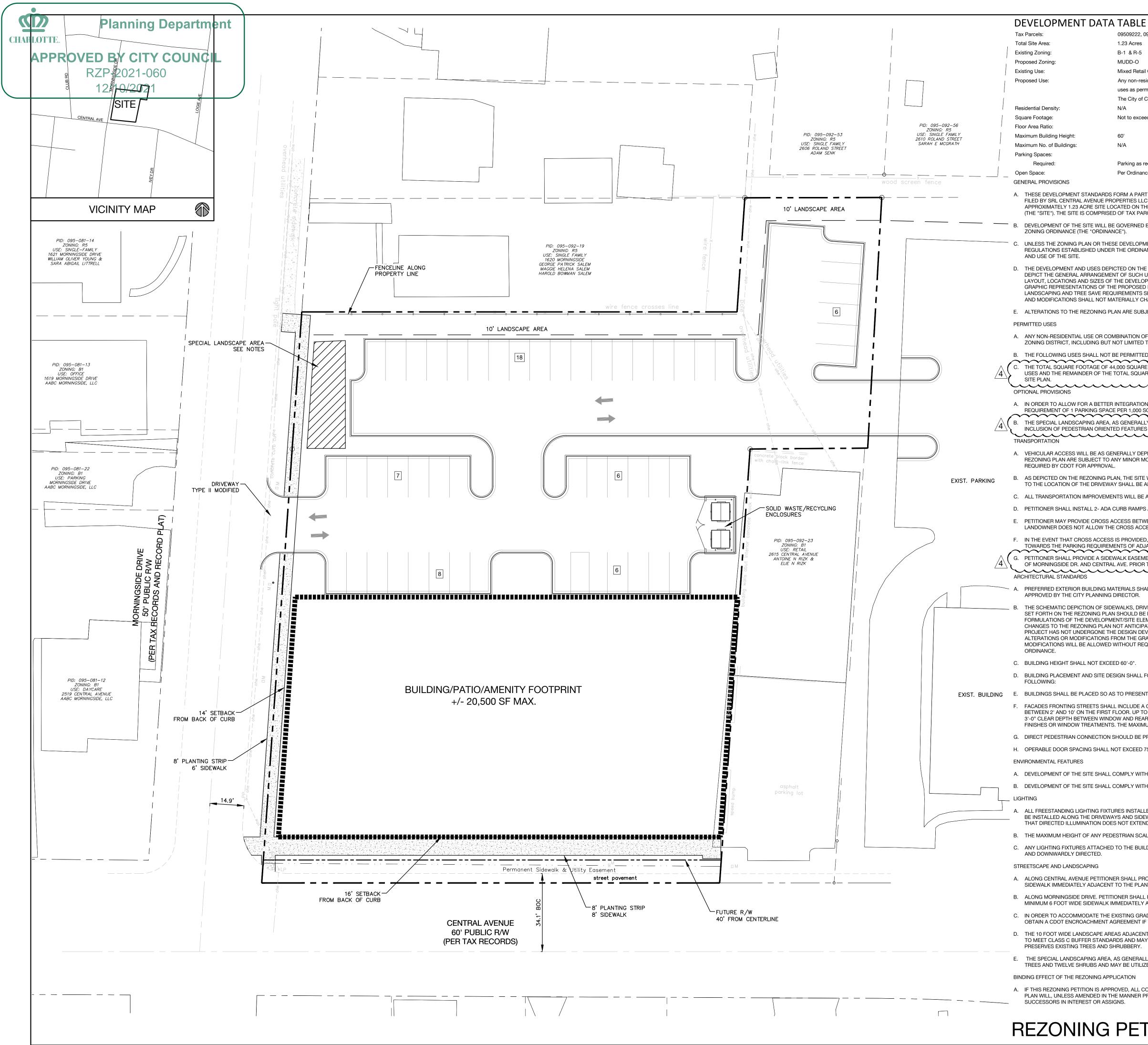
Charlotte Water has sanitary sewer system infrastructure accessible for the rezoning boundary via an existing 8-inch gravity sewer main located along Central Ave.

Development is encouraged to contact Installation Development Services at (704) 336-5499 to review service connection design requirements and City Ordinances (e.g., backflow, separate meter Ordinance, public/private pipeline extensions). Comments: Will need to acquire CAP. Has direct access to water & sewer. Will need to send plans to Charlotte Water for private water permitting. Will need to send plans to NCDEQ for private sewer permitting.

Reservation of sanitary sewer system capacity through the Charlotte Water's Capacity Assurance Program is the responsibility of the customer/development. Contact the New Services Group at (704) 432-2854 for further information on reserving capacity up to 24 months.

This sheet indicates the findings of a cursory review of Utilities System Information for the presence or absence of public water or wastewater infrastructure only. The findings do not imply available capacity in water and sanitary sewer lines, pump stations, or treatment facilities. Reservation of capacity is achieved through the Charlotte Water's Capacity Assurance Program.





<form></form>	The City of Charlotte Zoning Ordinance. N/A Not to exceed 44 ,000 sq ft 60' N/A Parking as required per Section 9.8507 Per Ordinance	URBAN DESIGN PARTNERS 1213 w morehead st suite 450 charlotte, nc 28208 urbandesignpartners.com P 704.334.3303 nc firm no: P-0418 sc coa no: C-03044		
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<form></form>	E TOTAL SQUARE FOOTAGE MAY BE ANY COMBINATION OF OTHER ALLOWABLE USES PURSUANT TO THE ORDINANCE AND THE CONDITIONS OF THE R INTEGRATION WITH ADJACENT RESIDENTIAL AND EXISTING AREA COMMERCIAL PARKING, PARKING REQUIREMENTS SHALL BE REDUCED TO A CE PER 1,000 SQ FT FOR ALL USES. , AS GENERALLY SHOWN ON THE SITE PLAN, SHALL INCLUDE AT LEAST TWO TREES AND TWELVE SHRUBS AND MAY BE UTILIZED FOR THE	Cen		Central
. COMPLY WITH CHARLOTTE PCS0. TURES INSTALLED ON THE SITE (EXCLUDING LOWER, DECORATIVE LIGHTING THAT MAY MAYS AND SIDEWALKS AS LANDSCAPING LIGHTING SHALLE FULLY CAPPED AND SHIELDED AND THE ILLUMINATION DOWNWARDLY DIRECTED SO INTO THE SITE (EXCLUDING LIGHTING STALLE FULLY CAPPED AND SHIELDED AND THE ILLUMINATION DOWNWARDLY DIRECTED SO DESTRIAN SCALE, FREESTANDING LIGHTING STRUP SET ALONG THE SITE, INCLUDING ITS BASE, SHALL NOT EXCEED FIFTEEN (15) FEET. D TO THE BUILDING TO BE CONSTRUCTED ON THE SITE SHALL BE DECORATIVE, CAPPED NERS SHALL PROVIDE A MINIMUM & FOOT WIDE PLANTING STRIP SET ALONG THE BACK OF THE EXISTING CURB AND A MINIMUM & FOOT WIDE INTO THE PLANTING STRIP AS GENERALLY SHOWN ON THE REZONING PLAN. TOOMER SHALL PROVIDE A MINIMUM & FOOT WIDE PLANTING STRIP SET ALONG THE BACK OF THE EXISTING CURB AND A MINIMUM & FOOT WIDE INTO THE PLANTING STRIP AS GENERALLY SHOWN ON THE REZONING PLAN. INDOMES MALL PROVIDE A MINIMUM B TOOT WIDE PLANTING STRIP SET ALONG THE BACK OF THE EXISTING CURB ALONG CENTRAL AVENUE AND ARGEMENT IF THE RETAINING WALL IS WITHIN THE PUBLIC RIGHT OF WAY. REAS ADJACENT TO THE LANTING STRIP SET ALONG THE BACK OF THE EXISTING CURB ALONG SCAPING AND MAY BUT UTLIZED AND MINIMUM ASTOT WIDE THAT PHELING STRIP, PROVIDED THAT PETITIONER ARGEMENT IF THE RETAINING WALL IS WITHIN THE PUBLIC RIGHT OF WAY. RAS BERNER.VL AS GENERALLY SHOWN ON THE ENDURGATING STRIP SET ALONG THE STANDARD. HOWEVER, ANY PENCING SHALL NOT BE REQUIRED TO A CLAR WAY BUT UTLIZED AND MEDIATELY ADJACENT TO THE LOT LINES. THE LANDSCAPING AREAS MAY BE MODIFIED FROM STANDARDS TO SHRUBERY. AS GENERALLY SHOWN ON THE STRE LINCLUSS AND ARDSCAPING REAS MA	ENFRALLY DEPICTED ON THE REZONING PLAN. THE PLACEMENT AND CONFIGURATION OF THE VEHICULAR ACCESS POINT SHOWN ON THE ANY MINOR MODIFICATIONS REQUIRED TO ACCOMMODATE FINAL STE AND CONSTRUCTION PLANS AND DESIGNS, AND TO ANY ADJUSTMENTS AL. VIAN, THE SITE WILL BE SERVED BY A PRIVATE DRIVEWAY, AND MINOR ADJUSTMENT AY SHALL BE ALLOWED DURING THE CONSTRUCTED BEFORE THE SITE'S FIRST BUILDING CERTIFICATE OF OCCUPANCY IS ISSUED. A CURB RAMPS AT THE CENTRAL AVENUE AND MORNINGSIDE DRIVE INTERSECTION PER ADA LAW AND PROWAG STANDARDS. ACCESS BETWEEN THE SITE AND THE ADJACENT PARCEL 09508224, AS GENERALLY SHOWN ON THE SITE PAUL, HOWEVER, IF THE ADJACENT 4C CROSS ACCESS, THEN CROSS ACCESS SHALL NOT BE REQUIRED AS A PART OF THE LAND DEVELOPMENT REVIEW PROCESS. IS IS PROVIDED, PARKING IN EXCESS OF REQUIREMENTS ON SITE MAY BE APPLIED MENTS OF ADJACENT PARCELS WHICH ARE CONNECTED VIA THE GROSS ACCESS PROVIDED. MENTS OF ADJACENT PARCELS WHICH ARE CONNECTED VIA THE CROSS ACCESS PROVIDED. MENTS OF ADJACENT PARCELS WHICH ARE CONNECTED VIA THE CROSS ACCESS PROVIDED. MENTS OF THE ISSUED OF BRICK, CEDAR SHAKE, WOOD SIDING, FIBER CEMENT SIDING, STUCCO, MASONRY AND/OR OTHER MATERIAL AVEF PRIOR TO THE ISSUED OF BRICK, CEDAR SHAKE, WOOD SIDING, FIBER CEMENT SIDING, STUCCO, MASONRY AND/OR OTHER MATERIAL AVEF PRIOR TO THE ISSUED OF BRICK, CEDAR SHAKE, WOOD SIDING, FIBER CEMENT SIDING, STUCCO, MASONRY AND/OR OTHER MATERIAL DEWALKS, DRIVEWAYS, STREETS AND OTHER DEVELOPMENT MATTERS AND SITE ELEMENTS (COLLECTIVELY "THE DEVELOPMENT/SITE ELEMENTS") NOT ANTICIPATED BY THE REZONING PLAN WILL BE REVIEWED AND APPROVED AS ALLOWED BY SECTION AGO OF THE ORDINANCE. SINCE THE DEWALKS, DRIVEWAYS, STREETS AND OTHER DAVELOPMENT MATTERS AND SITE ELEMENTS (COLLECTIVELY "THE DEVELOPMENT/SITE ELEMENTS") NOT ANTICIPATED BY THE REZONING PLAN WILL BE REVIEWED AND APPROVED AS ALLOWED BY SECTION AGO OF THE ORDINANCE. SINCE THE DEWALKS, DRIVEWAYS, STREETS AND OTHER DEVELOPMENT MATTERS AND SITE ELEMENTS (COLLECTIVELY THE DEVELOPMENTS THE LEMENTS IN THE ADMI	09 Central Avenu		oning P
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1.23 Acres B-1 & R-5

MUDD-O

09509222, 09509221, & 09509220

Mixed Retail Office and Gas Station

Any non-residential use or combination of uses and accessory

uses as permitted in the Mixed Use Development District of