

**SITE DEVELOPMENT DATA**

TAX PARCEL ID# S 145-255-01  
 JURISDICTION CHARLOTTE  
 ACREAGE ±13.27 ACRES  
 AREA PLANS SCALEYBARK TRANSIT STATION AREA PLAN  
 EXISTING USES VACANT  
 PROPOSED USE UP TO 94 FOR-SALE SINGLE-FAMILY ATTACHED (TOWNHOME) DWELLING UNITS  
 MAXIMUM BUILDING HEIGHT UP TO FORTY FEET (40) FEET, AS MEASURED PER THE ORDINANCE  
 PROPOSED UNITS 94 TOWNHOMES  
 PROPOSED DUA 7.08 UNITS PER ACRE

OPEN SPACE  
 OPEN SPACE REQUIRED 400 SF PER UNIT OR 10% SITE AREA COMMON OPEN SPACE  
 OPEN SPACE PROVIDED 10% SITE AREA  
 1.3AC (56,628SF)

TREE SAVE  
 TREE SAVE REQUIRED 15% (± 1.99 AC)  
 TREE SAVE PROVIDED 15% MIN.

ZONING CODE SUMMARY  
 EXISTING ZONING B-8  
 PROPOSED ZONING UR-2 (CD)

SETBACKS AND BUFFERS  
 SIDE YARD 5'  
 REAR YARD 10'

PARKING  
 PARKING WILL BE PROVIDED PER ORDINANCE

**I. GENERAL PROVISIONS**

- THESE DEVELOPMENT STANDARDS FORM A PART OF THE REZONING PLAN ASSOCIATED WITH THE REZONING PETITION FILED BY TRIPONTE HOMES (THE "PETITIONER") TO ACCOMMODATE THE DEVELOPMENT OF A SINGLE-FAMILY ATTACHED (TOWNHOME) RESIDENTIAL COMMUNITY ON THAT APPROXIMATELY 13.94-ACRE SITE LOCATED AT 150 ORCHARD CIRCLE, MORE PARTICULARLY DEPICTED ON THE REZONING PLAN (THE "SITE"). THE SITE IS COMPRISED OF TAX PARCEL NUMBER 145-255-01.
- DEVELOPMENT OF THE SITE WILL BE GOVERNED BY THE REZONING PLAN, THESE DEVELOPMENT STANDARDS AND THE APPLICABLE PROVISIONS OF THE CITY OF CHARLOTTE ZONING ORDINANCE (THE "ORDINANCE").
- UNLESS THE REZONING PLAN OR THESE DEVELOPMENT STANDARDS ESTABLISH MORE STRINGENT STANDARDS, THE REGULATIONS ESTABLISHED UNDER THE ORDINANCE FOR THE UR-2 ZONING DISTRICT SHALL GOVERN THE DEVELOPMENT AND USE OF THE SITE.
- THE DEVELOPMENT AND STREET LAYOUT DEPICTED ON THE REZONING PLAN ARE SCHEMATIC IN NATURE AND ARE INTENDED TO DEPICT THE GENERAL ARRANGEMENT OF SUCH USES AND IMPROVEMENTS ON THE SITE. ACCORDINGLY, THE ULTIMATE LAYOUT, LOCATIONS AND SIZES OF THE DEVELOPMENT AND SITE ELEMENTS DEPICTED ON THE REZONING PLAN ARE GRAPHIC REPRESENTATIONS OF THE PROPOSED DEVELOPMENT AND SITE ELEMENTS, AND THEY MAY BE ALTERED OR MODIFIED IN ACCORDANCE WITH THE SETBACK, YARD, LANDSCAPING AND TREE SAVE REQUIREMENTS SET FORTH ON THIS REZONING PLAN AND THE DEVELOPMENT STANDARDS, PROVIDED, HOWEVER, THAT ANY SUCH ALTERATIONS AND MODIFICATIONS SHALL BE MINOR IN NATURE AND NOT MATERIALLY CHANGE THE OVERALL DESIGN INTENT DEPICTED ON THE REZONING PLAN.
- FUTURE AMENDMENTS TO THE REZONING PLAN AND/OR THESE DEVELOPMENT STANDARDS MAY BE APPLIED FOR BY THE THEN OWNER(S) OF THE SITE IN ACCORDANCE WITH THE PROVISIONS OF CHAPTER 6 OF THE ORDINANCE. MINOR ALTERATIONS TO THE REZONING PLAN ARE SUBJECT TO SECTION 6.207 OF THE ORDINANCE.

**II. PERMITTED USES**

THE SITE MAY BE DEVOTED ONLY TO A RESIDENTIAL COMMUNITY CONTAINING A MAXIMUM OF NINETY-FOUR (94) SINGLE-FAMILY ATTACHED DWELLING UNITS AND ANY INCIDENTAL AND ACCESSORY USES RELATING THERETO THAT ARE PERMITTED IN THE UR-2 ZONING DISTRICT.

**III. TRANSPORTATION**

1. VEHICULAR ACCESS TO THE SITE SHALL BE AS GENERALLY DEPICTED ON THE REZONING PLAN. THE PLACEMENT AND CONFIGURATION OF THE VEHICULAR ACCESS POINTS ARE SUBJECT TO ANY MINOR MODIFICATIONS REQUIRED TO ACCOMMODATE FINAL SITE AND CONSTRUCTION PLANS AND DESIGNS AND AS

REQUIRED FOR APPROVAL BY THE CHARLOTTE DEPARTMENT OF TRANSPORTATION (CDOT).

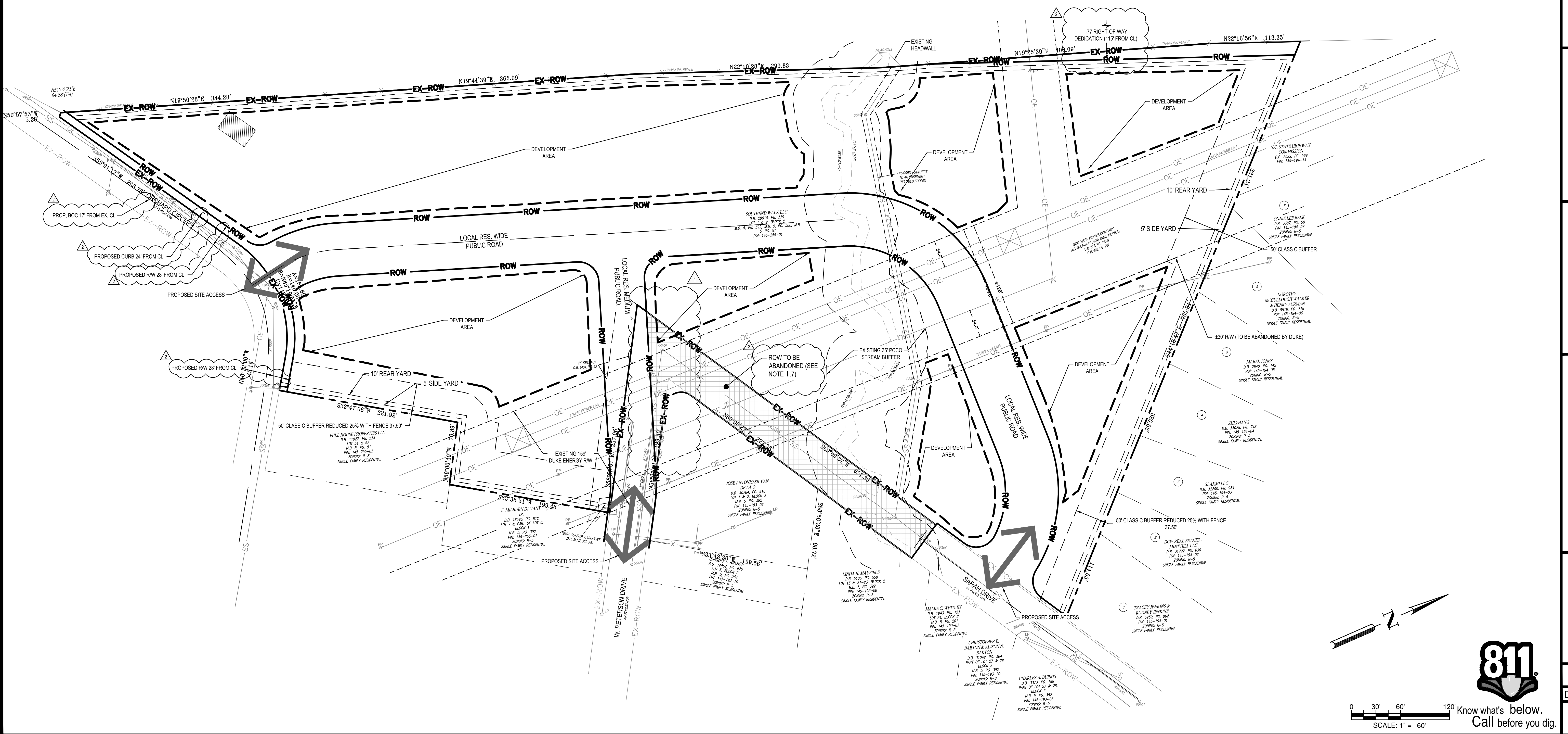
- AS DEPICTED ON THE REZONING PLAN, THE SITE WILL BE SERVED BY INTERNAL PUBLIC STREETS AND PRIVATE ALLEYS, AND MINOR ADJUSTMENTS TO THE LOCATION OF THESE STREETS/ALLEYS SHALL BE ALLOWED DURING THE CONSTRUCTION PERMITTING PROCESS IN COORDINATION WITH CDOT.
- PETITIONER SHALL PROVIDE INTERNAL SIDEWALKS AND PEDESTRIAN CONNECTIONS ON THE SITE AS GENERALLY DEPICTED ON THE REZONING PLAN. INTERNAL SIDEWALKS MAY MEANDER TO SAVE EXISTING TREES.
- PETITIONER SHALL POST 25 MPH SPEED LIMIT MUTCD SIGNS ON NEW PROPOSED PUBLIC STREETS ON THE SITE.
- PETITIONER SHALL DEDICATE ALL RIGHTS-OF-WAY WHERE NECESSARY, IN FEE SIMPLE CONVEYANCE TO THE CITY OF CHARLOTTE PRIOR TO THE ISSUANCE OF THE SITE'S FIRST BUILDING CERTIFICATE OF OCCUPANCY.
- UNLESS STATED OTHERWISE HEREIN, THE PETITIONER SHALL ENSURE THAT ALL TRANSPORTATION IMPROVEMENTS ARE SUBSTANTIALLY COMPLETED PRIOR TO THE ISSUANCE OF THE SITE'S FIRST BUILDING CERTIFICATE OF OCCUPANCY.
- WITH RESPECT TO THE AREA LABELED AS "POTENTIAL ABANDONMENT," PETITIONER WILL COMPLETE AND SUBMIT THE RIGHT-OF-WAY ABANDONMENT PETITION FORM TO CDOT FOR REVIEW. THE DECISION OF THIS REZONING PETITION DOES NOT CORRELATE WITH THE DECISION OF THE POSSIBLE ABANDONMENT, AS THIS DECISION IS ISSUED WITHIN THE SEPARATE RIGHT-OF-WAY ABANDONMENT PROCESS THAT IS CONTROLLED BY NORTH CAROLINA GENERAL STATUTES.

**IV. ARCHITECTURAL STANDARDS**

- PREFERRED EXTERIOR BUILDING MATERIALS: ALL PRINCIPAL AND ACCESSORY BUILDINGS SHALL BE COMPRISED OF A COMBINATION OF PORTIONS OF BRICK, BRICK VENEER, NATURAL STONE (OR ITS SYNTHETIC EQUIVALENT), STUCCO, CEMENTITIOUS SIDING, FIBER CEMENT ("HARDIPLANK") AND/OR OTHER MATERIALS APPROVED BY THE PLANNING DIRECTOR.
- PROHIBITED EXTERIOR BUILDING MATERIALS: (I) VINYL SIDING, EXCLUDING VINYL HANDRAILS, WINDOWS, SOFFITS, DOORS, GARAGE DOORS, RAILINGS, OR TRIM, AND (II) CONCRETE MASONRY UNITS NOT ARCHITECTURALLY FINISHED.
- PITCHED ROOFS, IF PROVIDED, SHALL BE SYMMETRICALLY SLOPED NO LESS THAN 5:12, EXCEPT THAT ROOFS FOR PORCHES AND ATTACHED SHEDS MAY BE NO LESS THAN 2:12, UNLESS A FLAT ROOF ARCHITECTURAL STYLE IS EMPLOYED.
- CORNER/END UNITS FRONTING PUBLIC STREETS SHALL HAVE ENHANCED SIDE ELEVATIONS WITH A FRONT OR REAR STOOP AND/OR PORCH OR OTHERWISE PROVIDE WINDOWS OR OTHER ARCHITECTURAL DETAILS TO LIMIT THE MAXIMUM BLANK WALL EXPANSE TO TWENTY (20) FEET ON EACH LEVEL OF THE UNIT. IF A

PORCH IS UTILIZED, IT MAY BE LOCATED ON THE SECOND STORY OF SUCH UNIT.

- FOR ALL UNITS, PETITIONER SHALL PROVIDE BLANK WALL PROVISIONS THAT LIMIT THE MAXIMUM BLANK WALL EXPANSE TO TWENTY (20) FEET ON ALL BUILDING LEVELS, INCLUDING BUT NOT LIMITED TO DOORS, WINDOWS, AWNINGS, MATERIAL OR COLOR CHANGES, AND/OR ARCHITECTURAL DESIGN ELEMENTS.
- ROOFTOP TERRACES MAY BE INSTALLED ON ANY/ALL UNITS ON THE SITE, AT THE PETITIONER'S OPTION.
- ALL TOWNHOME UNITS SHALL BE PROVIDED WITH A GARAGE FOR A MINIMUM OF ONE (1) CAR.
- THE PETITIONER MAY SUBDIVIDE THE SITE AND CREATE LOTS WITHIN THE SITE WITH NO SIDE OR REAR YARDS AS PART OF A UNIFIED DEVELOPMENT PLAN.
- ENVIRONMENTAL FEATURES
  - THE PETITIONER SHALL COMPLY WITH THE CHARLOTTE CITY COUNCIL APPROVED AND ADOPTED POST CONSTRUCTION STORMWATER ORDINANCE, THE LOCATION, SIZE, AND TYPE OF STORM WATER MANAGEMENT SYSTEMS DEPICTED ON THE REZONING PLAN ARE SUBJECT TO REVIEW AND APPROVAL AS PART OF THE FULL DEVELOPMENT PLAN SUBMITTAL AND ARE NOT IMPLICITLY APPROVED WITH THIS REZONING. ADJUSTMENTS MAY BE NECESSARY IN ORDER TO ACCOMMODATE ACTUAL STORM WATER TREATMENT REQUIREMENTS AND NATURAL SITE DISCHARGE POINTS.
  - THE PETITIONER SHALL COMPLY WITH THE CHARLOTTE TREE ORDINANCE.
- LIGHTING
  - ALL FREESTANDING LIGHTING FIXTURES INSTALLED ON THE SITE GREATER THAN TWENTY ONE (21) FEET IN TOTAL HEIGHT (I.E. EXCLUDING DECORATIVE LIGHTING LESS THAN 21' IN HEIGHT THAT MAY BE INSTALLED ALONG THE DRIVEWAYS AND SIDEWALKS AS PEDESTRIAN/LANDSCAPING LIGHTING) SHALL BE FULLY CAPPED AND SHIELDED AND THE ILLUMINATION DOWNWARDLY DIRECTED.
- BINDING EFFECT OF THE REZONING DOCUMENTS AND DEFINITIONS
  - IF THIS REZONING PETITION IS APPROVED, ALL CONDITIONS APPLICABLE TO DEVELOPMENT OF THE SITE IMPOSED UNDER THE REZONING PLAN AND THESE DEVELOPMENT STANDARDS WILL, UNLESS AMENDED IN THE MANNER PROVIDED UNDER THE ORDINANCE, BE BINDING UPON AND INURE TO THE BENEFIT OF THE PETITIONER AND SUBSEQUENT OWNERS OF THE SITE AND THEIR RESPECTIVE SUCCESSORS IN INTEREST AND ASSIGNS.
  - THROUGHOUT THESE DEVELOPMENT STANDARDS, THE TERMS, "PETITIONER" AND "OWNER" OR "OWNERS" SHALL BE DEEMED TO INCLUDE THE HEIRS, DEVISEES, PERSONAL REPRESENTATIVES, SUCCESSORS IN INTEREST AND ASSIGNS OF THE PETITIONER OR THE OWNER OR OWNERS OF ANY PART OF THE SITE FROM TIME TO TIME WHO MAY BE INVOLVED IN ANY FUTURE DEVELOPMENT THEREOF.



REV	DATE	DESCRIPTION
1	06/11/2020	3RD REZONING SUBMITTAL
2	06/11/2020	2ND REZONING SUBMITTAL

DESIGNED BY:  
 DRAWN BY:  
 REVIEWED BY:

**BCE**  
 121 W. TRADE STREET, SUITE 1030  
 CHARLOTTE, NC 28202  
 www.bceinc.com  
 NC LICENSE #C-4397

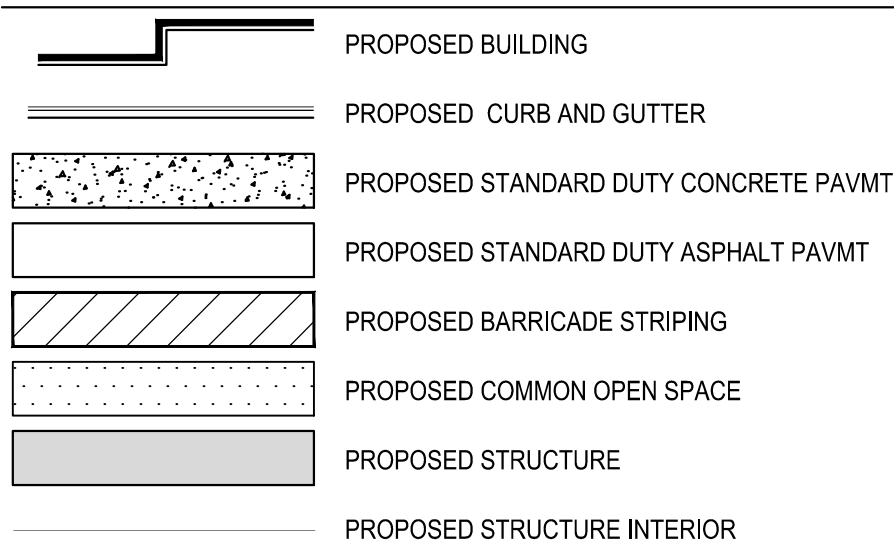
**TRI Pointe Homes**  
 3436 TORINGDON WAY, SUITE 210  
 CHARLOTTE, NC 28202

**SOUTHWALK**  
 150 ORCHARD CIRCLE  
 CHARLOTTE, NORTH CAROLINA 28217

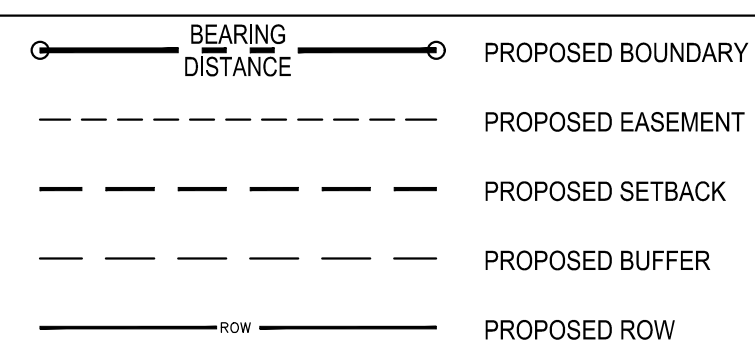
**TECHNICAL DATA SHEET**

FILE NUMBER:  
7252-00  
 DATE: 06/11/2020  
 RZ-1

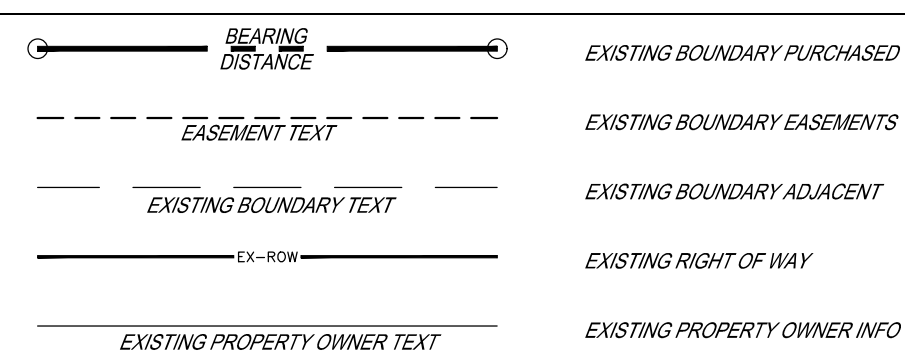
**SITE PLAN LEGEND**



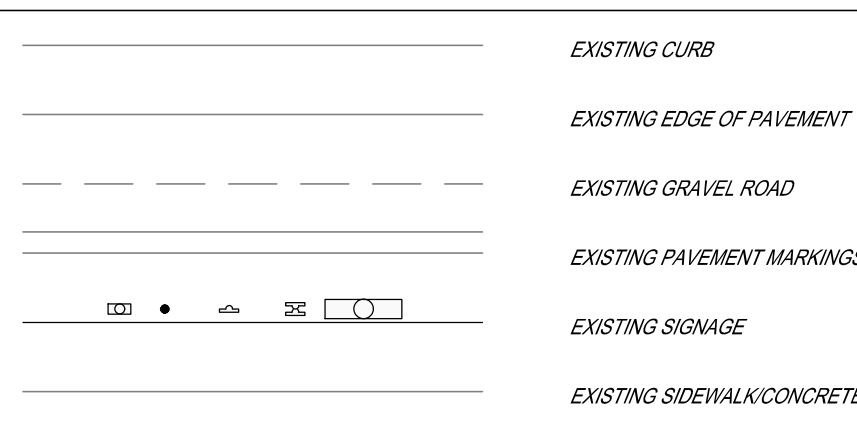
**PROPOSED BOUNDARY LEGEND**



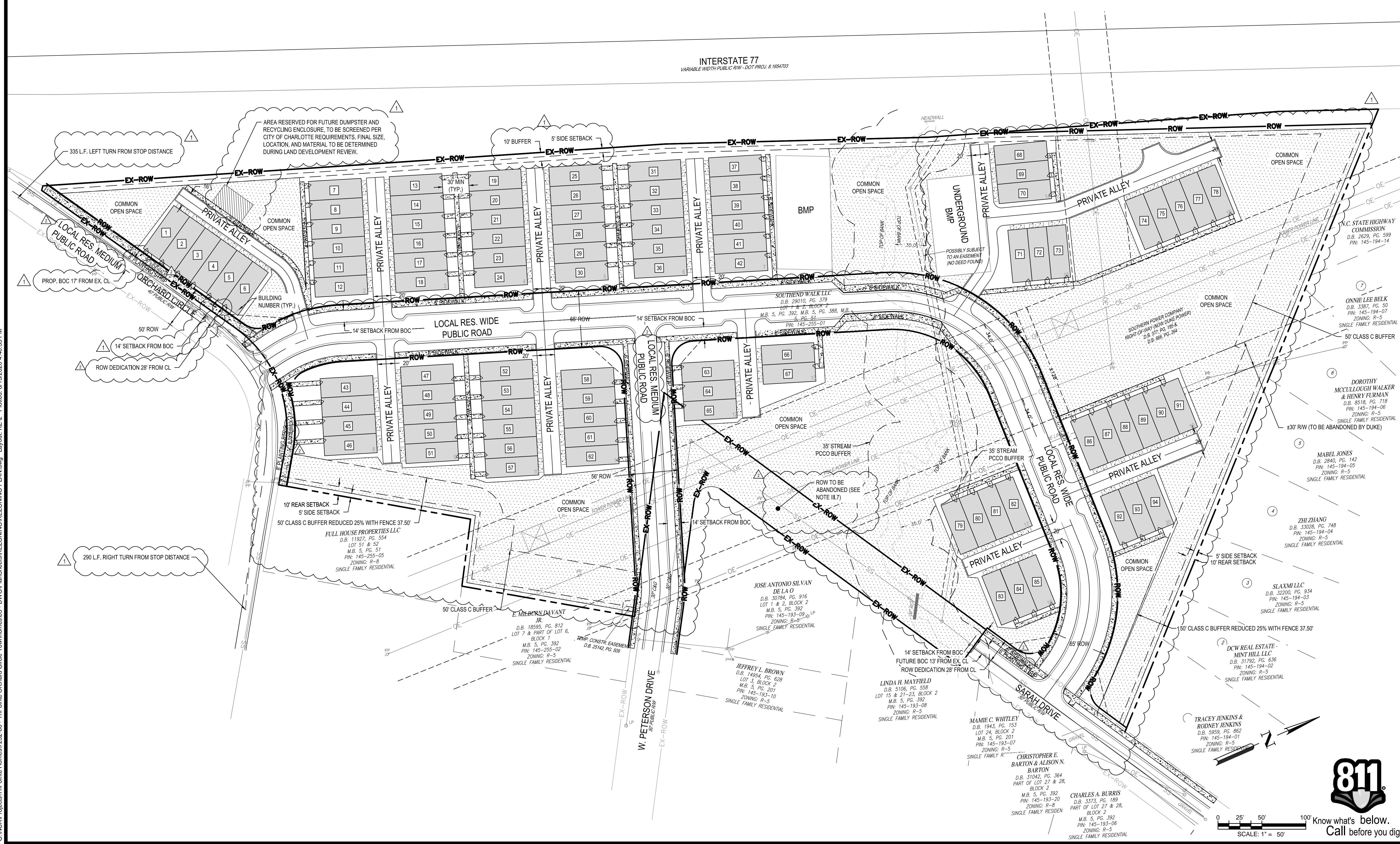
**EXISTING BOUNDARY LEGEND**



**EXISTING PAVEMENT LEGEND**



INTERSTATE 77  
VARIABLE WIDTH PUBLIC ROW - DOT PROJ. & 1654703



SCALE: 1" = 50'

REV	DATE	DESCRIPTION
1	06/11/2020	3RD REZONING SUBMITTAL
2	06/11/2020	2ND REZONING SUBMITTAL

DESIGNED BY:  
DRAWN BY:  
REVIEWED BY:

**BCE**  
121 W. TRADE STREET, SUITE 1030  
CHARLOTTE, NC 28202  
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NC LICENSE #C-4387

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CHARLOTTE, NC 28202

**SOUTHEND WALK**  
150 ORCHARD CIRCLE  
CHARLOTTE, NORTH CAROLINA 28217

**SCHEMATIC SITE PLAN**

FILE NUMBER:  
7252-00

DATE: 06/11/2020

RZ-2

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