

1. GENERAL PROVISIONS

- a) THESE DEVELOPMENT STANDARDS FORM A PART OF THE REZONING PLAN ASSOCIATED WITH THE REZONING PETITION FILED BY REDWOOD USA TO ACCOMMODATE THE DEVELOPMENT OF A MULTI-FAMILY RESIDENTIAL COMMUNITY ON THAT APPROXIMATELY 21.08 ACRE SITE LOCATED ON HARRIS HOUSTON ROAD, WHICH SITE IS MORE PARTICULARLY DEPICTED ON THE REZONING PLAN (THE "SITE"). THE SITE IS COMPRISED OF TAX PARCEL NOS. 05107104, 05107109, 05128180.
- b) DEVELOPMENT OF THE SITE WILL BE GOVERNED BY THE REZONING PLAN, THESE DEVELOPMENT STANDARDS AND THE APPLICABLE PROVISIONS OF THE CITY OF CHARLOTTE ZONING ORDINANCE (THE "ORDINANCE"). UNLESS THE REZONING PLAN OR THESE DEVELOPMENT STANDARDS ESTABLISH MORE STRINGENT STANDARDS, THE REGULATIONS ESTABLISHED UNDER THE ORDINANCE FOR THE R-8MF ZONING DISTRICT SHALL GOVERN THE DEVELOPMENT AND USE OF THE SITE.
- c) THE DEVELOPMENT DEPICTED ON THE REZONING PLAN IS SCHEMATIC IN NATURE AND INTENDED TO DEPICT THE GENERAL ARRANGEMENT OF USES AND IMPROVEMENTS ON THE SITE. ACCORDINGLY, THE CONFIGURATION, PLACEMENT AND SIZE OF THE PRINCIPAL BUILDING FOOTPRINT AS WELL AS THE INTERNAL DRIVES AND PARKING AREAS DEPICTED ON THE REZONING PLAN ARE SCHEMATIC IN NATURE AND, SUBJECT TO THE TERMS OF THESE DEVELOPMENT STANDARDS AND THE ORDINANCE, ARE SUBJECT TO MINOR ALTERATIONS OR MODIFICATIONS DURING THE DESIGN DEVELOPMENT AND CONSTRUCTION DOCUMENT PHASES.
- d) FUTURE AMENDMENTS TO THE REZONING PLAN AND/OR THESE DEVELOPMENT STANDARDS MAY BE APPLIED FOR BY THE THEN OWNER OR OWNERS OF THE SITE IN ACCORDANCE WITH THE PROVISIONS OF CHAPTER 6 OF THE ORDINANCE. ALTERATIONS TO THE REZONING PLAN ARE SUBJECT TO SECTION 6.207 OF THE ORDINANCE.
- e) DEDICATION AND FEE SIMPLE CONVEYANCE OF ALL RIGHTS OF WAY (IF APPLICABLE) TO THE CITY SHALL OCCUR BEFORE THE SITE'S FIRST BUILDING CERTIFICATE OF OCCUPANCY IS ISSUED.

2. PERMITTED USES

a) THE SITE MAY ONLY BE DEVOTED TO A MULTI-FAMILY RESIDENTIAL COMMUNITY CONTAINING A MAXIMUM OF 83 ATTACHED DWELLING UNITS, 1 DETACHED LEASING OFFICE/MAINTENANCE BUILDING AND TO ANY INCIDENTAL AND ACCESSORY USES RELATING THERETO THAT ARE ALLOWED IN THE R-8MF ZONING DISTRICT.

3. TRANSPORTATION

a) VEHICULAR ACCESS TO THE SITE WILL BE FROM HARRIS HOUSTON ROAD AS GENERALLY DEPICTED ON REZONING PLAN. THE PLACEMENT AND

- CONFIGURATION OF THE VEHICULAR ACCESS POINTS ARE SUBJECT TO ANY MINOR MODIFICATIONS REQUIRED TO ACCOMMODATE FINAL SITE AND CONSTRUCTION PLANS AND DESIGNS AND TO ANY ADJUSTMENTS REQUIRED FOR APPROVAL BY THE CHARLOTTE DEPARTMENT OF TRANSPORTATION (CDOT) AND/OR THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION (NCDOT) IN ACCORDANCE WITH APPLICABLE PUBLISHED STANDARDS.
- b) PETITIONER SHALL INSTALL PRIVATE ALLEY STREETS A - D IN THE LOCATIONS GENERALLY DEPICTED ON REZONING PLAN. NO PUBLIC STREETS ARE REQUIRED TO BE CONSTRUCTED ON SITE AS CONFIRMED BY THE CITY. PRIVATE ALLEYS SHALL BE MAINTAINED AND OWNED BY THE SITE DEVELOPER.
- c) PETITIONER SHALL WIDEN HARRIS HOUSTON ROAD 5. STREETSCAPE AND LANDSCAPING (NEAR SIDE ONLY) TO FOLLOW CLDSM STD. NO. U-07 ALONG PROPERTY FRONTAGE. IN LIEU OF PROVIDING A 5' BIKE LANE, PETITIONER TO PROVIDE 12' MULTI-USE PATH.
- d) ALL TRANSPORTATION IMPROVEMENTS WILL BE APPROVED AND CONSTRUCTED BEFORE THE SITE'S FIRST BUILDING CERTIFICATE OF OCCUPANCY IS ISSUED.
- e) THE SITE WILL UTILIZE SAID PRIVATE STREETS AND PRIVATE DRIVEWAY CONNECTIONS THAT WILL CONNECT TO HARRIS HOUSTON ROAD. DUE TO SITE FEATURES, NO FUTURE CONNECTIVITY TO ADJACENT PROPERTIES WILL BE REQUIRED, AS CONFIRMED BY THE CITY.
- PARKING FOR EACH INDIVIDUAL UNIT IS PROVIDED IN A TWO CAR GARAGE WITH AN ADDITIONAL TWO SPACES WITHIN THE DRIVEWAY AREA.
- THE ALIGNMENT OF THE INTERNAL DRIVEWAYS AND VEHICULAR CIRCULATION AREAS MAY BE MODIFIED BY THE PETITIONER TO ACCOMMODATE CHANGES FOR THE FINAL LAYOUT, TRAFFIC PATTERNS AND ANY ADJUSTMENTS REQUIRED FOR 6. ENVIRONMENTAL FEATURES APPROVAL BY CDOT OR NCDOT IN ACCORDANCE WITH APPLICABLE PUBLISHED STANDARDS, MODIFICATIONS SHALL BE MINOR.
- g) ALL TRANSPORTATION IMPROVEMENTS WILL BE APPROVED AND CONSTRUCTED BEFORE THE SITE'S FIRST BUILDING CERTIFICATE OF OCCUPANCY IS **ISSUED**

4. ARCHITECTURAL STANDARDS

- a) THE MAXIMUM ALLOWED HEIGHT IN FEET OF THE PRINCIPAL BUILDINGS TO BE CONSTRUCTED ON THE SITE SHALL BE 40'. BUILDING HEIGHT SHALL BE MEASURED AS PROVIDED IN THE ORDINANCE.
- b) TO PROVIDE PRIVACY, ALL RESIDENTIAL ENTRANCES WITHIN 15 FEET OF THE SIDEWALK MUST BE RAISED FROM THE AVERAGE SIDEWALK GRADE A MINIMUM OF 24 INCHES.
- c) PITCHED ROOFS, IF PROVIDED, SHALL BE SYMMETRICALLY SLOPED NO LESS THAN 5:12, EXCEPT THAT ROOFS FOR PORCHES AND ATTACHED SHEDS MAY BE NO LESS THAN 2:12, UNLESS A FLAT ROOF ARCHITECTURAL STYLE IS EMPLOYED.
- d) ALL CORNER/END UNITS THAT FACE A PUBLIC OR PRIVATE STREET SHOULD HAVE A PORCH OR STOOP THAT WRAPS A PORTION OF THE FRONT AND SIDE OF THE UNIT OR PROVIDE BLANK WALL PROVISIONS THAT LIMIT THE MAXIMUM BLANK WALL EXPANSE TO 10 FEET ON ALL BUILDING LEVELS.
- e) GARAGE DOORS VISIBLE FROM PUBLIC OR PRIVATE

- STREETS SHOULD MINIMIZE THE VISUAL IMPACT BY PROVIDING ADDITIONAL ARCHITECTURAL TREATMENTS SUCH AS TRANSLUCENT WINDOWS OR PROJECTING ELEMENTS OVER THE GARAGE DOOR OPENING.
- f) WALKWAYS SHOULD BE PROVIDED TO CONNECT ALL RESIDENTIAL ENTRANCES TO SIDEWALKS ALONG PUBLIC AND PRIVATE STREETS.
- g) TOWNHOUSE BUILDINGS FRONTING PUBLIC OR PRIVATE NETWORK REQUIRED STREETS SHOULD BE LIMITED TO 5 INDIVIDUAL UNITS OR FEWER. THE NUMBER OF INDIVIDUAL UNITS PER BUILDING SHOULD BE VARIED IN ADJACENT BUILDINGS IF MULTIPLE 5 UNITS BUILDINGS ARE ADJACENT.

- a) THE PETITIONER WILL PROVIDE AN 8' PLANTING STRIP AND A 12' MULTI-USE PATH (CLDSM STD NO. 10.42) FOR HARRIS HOUSTON ROAD AS GENERALLY DEPICTED ON THE REZONING PLAN.
- b) ENTRY FEATURES WITH LANDSCAPING AND PROJECT IDENTITY SIGNAGE WILL BE PROVIDED AT MAIN ENTRY POINTS ALONG HARRIS HOUSTON ROAD AT THE DEVELOPER'S DISCRETION.
- c) BUFFER PLANTINGS INSIDE AREA NOTED AS "100' PCCO BUFFER" CAN BE POTENTIALLY WAIVED WITH THIS DEVELOPMENT PROVIDED THAT THE PETITIONER DEDICATES THE FULL WIDTH OF PERIMETER BUFFER AREA TO MECKLENBURG COUNTY PARKS AND RECREATION FOR FUTURE GREENWAY USE.
- d) TREE SURVEY OF EXISTING R/W TREES WILL BE REQUIRED AT FIRST SUBMITTAL OF PLAN REVIEW. ALL TREES 8" DBH OR GREATER IN R/W MUST BE PRESERVED OR WILL REQUIRE MITIGATION.

- a) THE PETITIONER SHALL COMPLY WITH THE CHARLOTTE CITY COUNCIL APPROVED AND ADOPTED POST CONSTRUCTION STORMWATER ORDINANCE.
- b) THE LOCATION, SIZE AND TYPE OF STORM WATER MANAGEMENT SYSTEMS DEPICTED ON THE REZONING PLAN ARE SUBJECT TO REVIEW AND APPROVAL AS PART OF THE FULL DEVELOPMENT PLAN SUBMITTAL AND ARE NOT IMPLICITLY APPROVED WITH THIS REZONING. ADJUSTMENTS MAY BE NECESSARY IN ORDER TO ACCOMMODATE **ACTUAL STORM WATER TREATMENT** REQUIREMENTS AND ACTUAL SITE DISCHARGE POINTS.
- c) THE DEVELOPMENT OF THIS SITE WILL COMPLY WITH THE CITY OF CHARLOTTE TREE ORDINANCE.
- d) IT IS ANTICIPATED THAT SOLID WASTE AND RECYCLING COLLECTION SERVICES ARE TO BE PROVIDED BY A PRIVATE COLLECTION SERVICE AND IN THIS EVENT, ROLL-OUT BINS WILL BE PROVIDED FOR EACH DWELLING UNIT.
- e) DEVELOPMENT WITHIN THE SWIM/PCSO BUFFER SHALL BE COORDINATED WITH AND SUBJECT TO APPROVAL BY CHARLOTTE-MECKLENBURG STORM WATER SERVICES AND MITIGATED IF REQUIRED BY CITY ORDINANCE.

f) STREAM DELINEATION REPORTS ARE SUBJECT TO REVIEW AND APPROVAL BY CHARLOTTE STORMWATER SERVICES.

7. SIGNAGE

a) PRINCIPAL SIGNAGE FOR THE SITE SHALL CONSIST OF A MONUMENT STYLE SIGN TO BE LOCATED AT PROJECT ENTRY POINTS AND WILL BE IN ACCORDANCE WITH THE CHARLOTTE LAND DEVELOPMENT STANDARDS MANUAL AND PER ORDINANCE.

8. LIGHTING

- a) ALL FREESTANDING LIGHTING FIXTURES INSTALLED ON THE SITE (EXCLUDING LOWER, DECORATIVE LIGHTING THAT MAY BE INSTALLED ALONG THE DRIVEWAYS AND SIDEWALKS AND LANDSCAPING LIGHTING) SHALL BE FULLY CAPPED AND SHIELDED AND THE ILLUMINATION DOWNWARDLY DIRECTED SO THAT DIRECT ILLUMINATION DOES NOT EXTEND PAST ANY PROPERTY LINE OF THE SITE.
- b) THE MAXIMUM HEIGHT OF ANY FREESTANDING LIGHTING FIXTURE INSTALLED ON THE SITE SHALL BE 21 FEET FROM GRADE.
- c) ANY LIGHTING FIXTURES ATTACHED TO PRINCIPAL BUILDINGS TO BE CONSTRUCTED ON THE SITE SHALL BE DECORATIVE, CAPPED AND DOWNWARDLY DIRECTED.

9. BINDING EFFECT OF THE REZONING APPLICATION

i) IF THIS REZONING PETITION IS APPROVED, ALL CONDITIONS APPLICABLE TO THE DEVELOPMENT OF THE SITE IMPOSED UNDER THE REZONING PLAN WILL. UNLESS AMENDED IN THE MANNER PROVIDED UNDER THE ORDINANCE, BE BINDING UPON AND INURE TO THE BENEFIT OF THE PETITIONER AND SUBSEQUENT OWNERS OF THE SITE AND THEIR RESPECTIVE HEIRS, DEVISEES, PERSONAL REPRESENTATIVES, SUCCESSORS IN INTEREST OR ASSIGNS.

REVISIONS			
REV	DATE	COMMENT	DRAWN BY
1	8/13/20	CITY COMMENTS	ATZ DR



ISSUED FOR MUNICIPAL & GENCY REVIEW & APPROVAL

CAD I.D.: PROJECT:

REZONING PLANS

—— FOR ——



REDWOOD HARRIS HOUSTON

3223 HARRIS HOUSTON ROAD CITY OF CHARLOTTE CHARLOTTE, NC 28262

BOHLER//

1927 S. TRYON STREET, SUITE 310 **CHARLOTTE, NC 28203** Phone: (980) 272-3400 (980) 272-3401 NC@BohlerEng.com

CONDITIONAL **REZONING: DEVELOPMENT**

RZ2.0

STANDARDS

REVISION 1 - 8/13/20